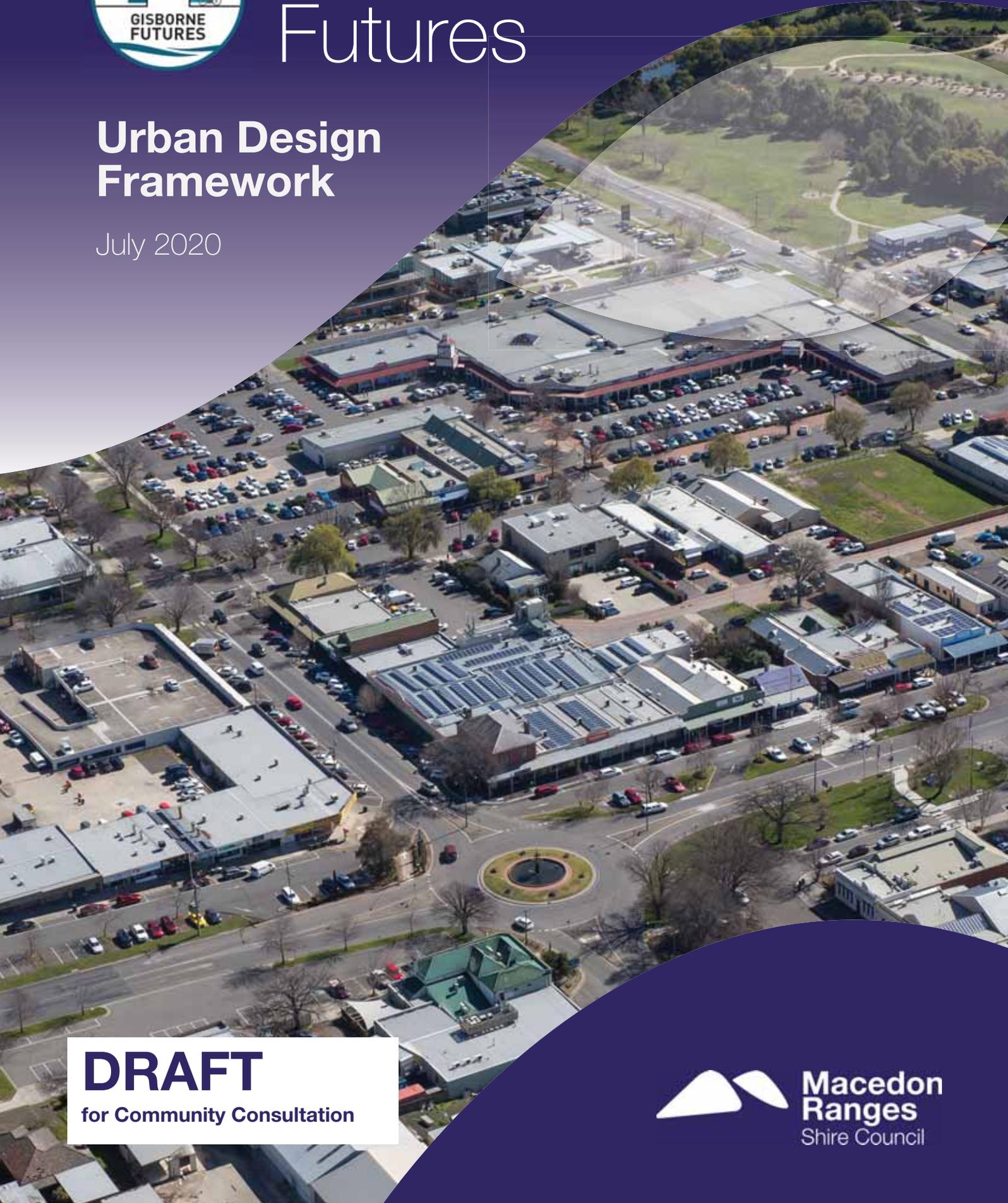




Gisborne Futures

Urban Design Framework

July 2020



DRAFT

for Community Consultation



**Macedon
Ranges**
Shire Council



Version Control

Date	Version	Purpose
March 2020	Version 1	Preliminary Draft for Internal review
May 2020	Version 2	Preliminary Draft for Community Consultation
July 2020	Version 3	Draft for Community Consultation

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Acknowledgements



The project team acknowledges the Traditional Owners of country throughout Australia, and specifically the Wurundjeri Woi wurrung People, and recognises their continuing connection to land, waters and culture. We pay our respects to their Elders past, present and emerging.

We thank the community groups and individuals, business owners and other community stakeholders who gave generous input to the Gisborne Futures Plan. Your feedback and ideas have been invaluable to the project.



Project Team

Macedon Ranges Shire Council prepared this report following the extensive background work, project management and technical analysis undertaken by Ethos Urban.

Ethos Urban

- Project coordination and management
- Community engagement
- Background and technical analysis
- Community infrastructure needs assessment
- Strategic planning and urban design



Urban Enterprise

- Economic & Employment Analysis
- Residential Land Supply and Demand Analysis



Extent Heritage

- Heritage Assessment



Wurundjeri Woi-wurrung Cultural Heritage Aboriginal Corporation

- Cultural Heritage Values Assessment



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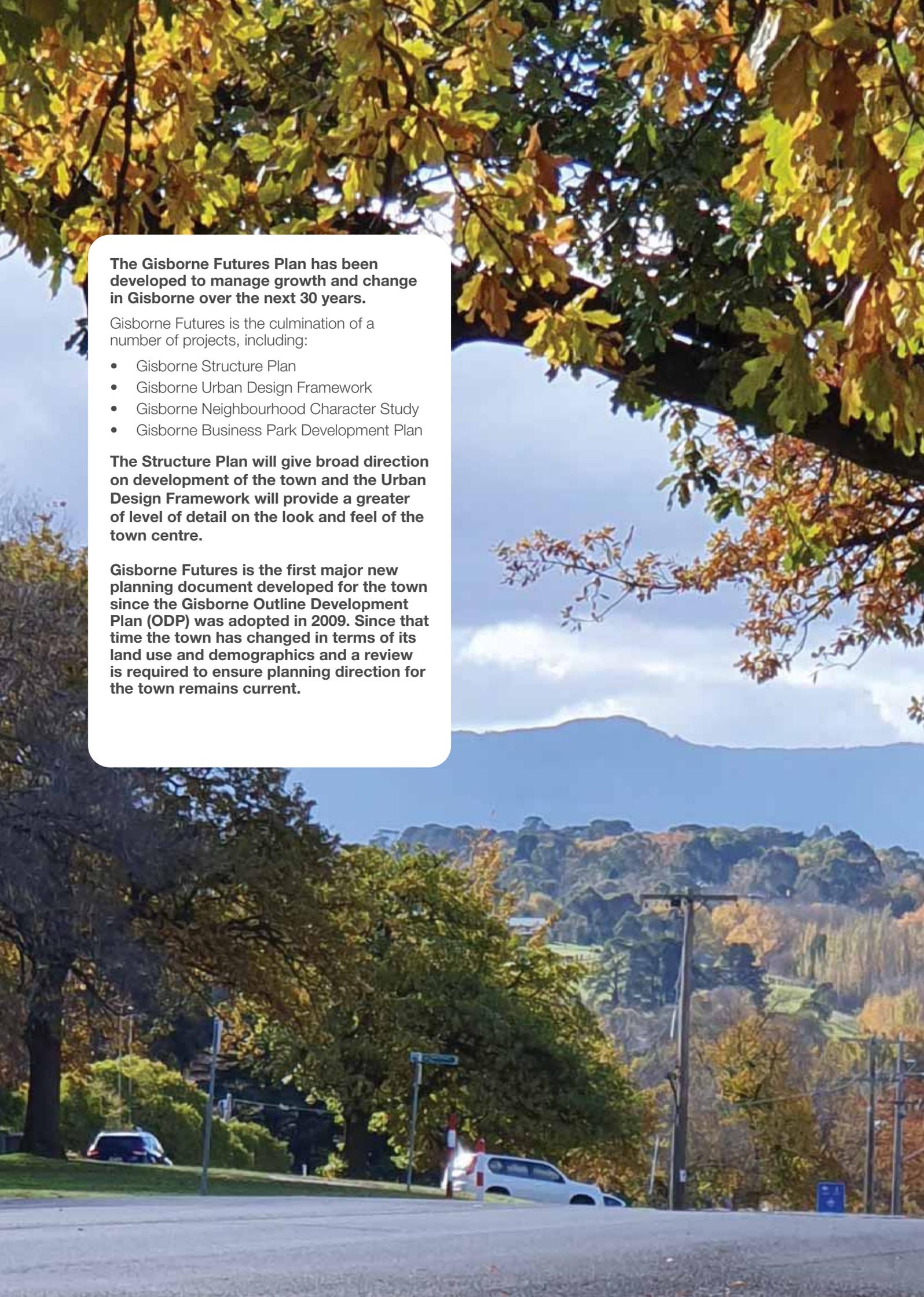
- Traffic & Transport Assessment
- Car Parking Precinct Plan
- Strategic Traffic Modelling



TGM Engineers

- Servicing Report





The Gisborne Futures Plan has been developed to manage growth and change in Gisborne over the next 30 years.

Gisborne Futures is the culmination of a number of projects, including:

- Gisborne Structure Plan
- Gisborne Urban Design Framework
- Gisborne Neighbourhood Character Study
- Gisborne Business Park Development Plan

The Structure Plan will give broad direction on development of the town and the Urban Design Framework will provide a greater level of detail on the look and feel of the town centre.

Gisborne Futures is the first major new planning document developed for the town since the Gisborne Outline Development Plan (ODP) was adopted in 2009. Since that time the town has changed in terms of its land use and demographics and a review is required to ensure planning direction for the town remains current.

1. About the Gisborne Futures Project

The Gisborne Futures project recognises the long-term implications for Gisborne in terms of capacity for change within the peri-urban influence of metropolitan Melbourne. The Plan considers housing development and the need for community and service infrastructure in tandem to meet the needs of the community, and provides direction for how these may be accommodated to respond to the highly valued characteristics of the town.

Consideration of neighbourhood and landscape character values, tree retention and preservation and enhancement of parks and open space throughout the town is included to ensure these are not compromised as the town continues to grow and change.

Within this context, the Gisborne Futures project seeks to identify and harness the opportunities provided by growth to ensure that it delivers positive outcomes for the township and the community. Setting a long-term vision for land development will help identify what social, community and physical infrastructure will be required for existing and future communities, and will plan to deliver them in a timely and coordinated manner.



The Gisborne Futures project comprises three key documents:

Gisborne Structure Plan

The Gisborne Structure Plan sets out the long-term strategic vision and action plan for the whole town including New Gisborne.

Gisborne Urban Design Framework

The Gisborne Urban Design Framework (UDF) sets out the direction for new development within the town centre, including buildings, streetscape works and public realm improvements.

Gisborne Neighbourhood Character Study

The Gisborne Neighbourhood Character Study identifies the precincts of Gisborne that share common neighbourhood characteristics and identifies planning controls to assist in achieving the precinct's preferred character

Planning for 2050

The Gisborne Futures project has been developed to manage growth and change in Gisborne over the next 30 years.

As a regional centre and as the primary town in the Macedon Ranges Shire where growth will be focused, we must ensure that sufficient land is set within the Gisborne township boundary to fulfil this role.

The year 2050 is not a date to aim for and it does not define a desired future population for the township. Rather, it has been set to ensure the sequential development of land occurs in an orderly fashion, and that all the essential infrastructure and services are available to communities as they are required.

.....

Gisborne Urban Design Framework

This report presents the Urban Design Framework (UDF) for community comment. It follows the Emerging Ideas phase completed in May 2019 and considers further research and the feedback received during that phase.

This Urban Design Framework provides built form and urban design guidelines and streetscape concept plans that maintain Gisborne’s village character through protection of landscape, built form and public realm elements that define the town, and ensure new development is respectful of the valued township character.

The UDF contains concept plans for potential improvements to public open spaces and street networks within the town centre that are proposed to enhance the pedestrian and urban environment.

Project Stages



The Gisborne Futures project is being prepared over six stages.

The project commenced with background research, data gathering and technical analysis to inform the input into the plans.

Following engagement with the community about issues and opportunities for the towns, ideas for the future of Gisborne/New Gisborne were presented back to the community for feedback and discussion through the Emerging Ideas Phase.

The preferred options for the Structure Plan, Urban Design Framework and Neighbourhood Character Study have been developed into draft plans for community consultation (current stage).

Preparation of a second set draft plans (Stage 5) will consider community comments and suggestions received and will include a detailed implementation plan. These will be exhibited to the community for comment again before the final Plans are prepared (Stage 6)

Implementation (Stage 7) will require seeking Council’s formal adoption of the plan which will be followed by a planning scheme amendment process to implement key policy and direction into the Macedon Ranges Planning Scheme.

2. Introduction



Maintain Gisborne’s unique village character through protection of landscape, built form and public realm elements that define the town, and ensure new development is respectful of the valued township character.

2.1. Town Character

Gisborne’s town centre is contained within the Jacksons Creek valley. The topographical change, wide streets lined by mature deciduous trees, mountain views to the north and south and generous open spaces are all important elements that contribute to Gisborne’s unique sense of place.

Diversion of the highway from the centre of town has provided opportunity for it to develop and maintain a ‘village’ character. Low-level lack of commercial development on the fringes of town, and a lack of larger allotments within town has avoided a proliferation of generic, large format retail which generally relies on high roadside exposure. Rather, a fine-grain pattern of local businesses has evolved, including hospitality, boutique retail, service and office uses. The local village characteristic is influenced by the compact nature of the town centre and physical features including:

- A pedestrian-scale streetscape with generous footpaths, verandahs and established street trees.
- A fine grain pattern of development that accommodates a diverse mix of local, small businesses.
- Minimal presence of large chain stores with generic branding and signage.
- Modest built form that does not dominate the streetscape or landscape setting.
- Community facilities, sporting grounds and parklands within the town centre.

2.2. Themes and Guiding Principles

The Gisborne Urban Design Framework establishes an integrated design vision for the desired future character of the town centre.

The aim of the UDF is to guide future development through objectives, planning and design requirements and guidelines that have been tailored to respond to the township character, while still allowing for a degree of development to ensure that sites are viable and attractive development opportunities. The UDF is structured around the following themes:

Land Use

- Strengthen the role of the town centre as Gisborne’s hub for community facilities and activities and provide opportunities for local economic development in all sectors, that will comprise a mix of thriving retail, commercial, service and health uses.

Entrances and Gateways

- Promote Gisborne as a ‘village in the valley’ through retention and enhancement of township edges and entrances.

Built Form

- Retain and enhance the village qualities of Gisborne’s town centre by encouraging excellence in architectural and urban design that responds to the character, history and identity of Gisborne.
- Enhance the pedestrian experience and encourage activity, economic prosperity and social interaction within the town centre.

Movement & Access

- Provide streetscapes that are safe, interesting, connected and comfortable for pedestrians to access.
- Plan to upgrade and improve the function of roads, intersections, car parking and within the town centre.

Landscape and Public Realm

- Create attractive civic spaces with landscaping and amenities that exhibit high quality design and provide opportunity for outdoor interaction and passive use of streets.

3. Consultation Summary

The community has contributed to the development of Gisborne Futures through two rounds of consultation:

- Context and Technical Analysis in August 2018
- Emerging Ideas in May 2019

Feedback received during the previous two phases of consultation demonstrates that respondents strongly advocate for the retention and protection of the village character and the low scale feel of the town centre.

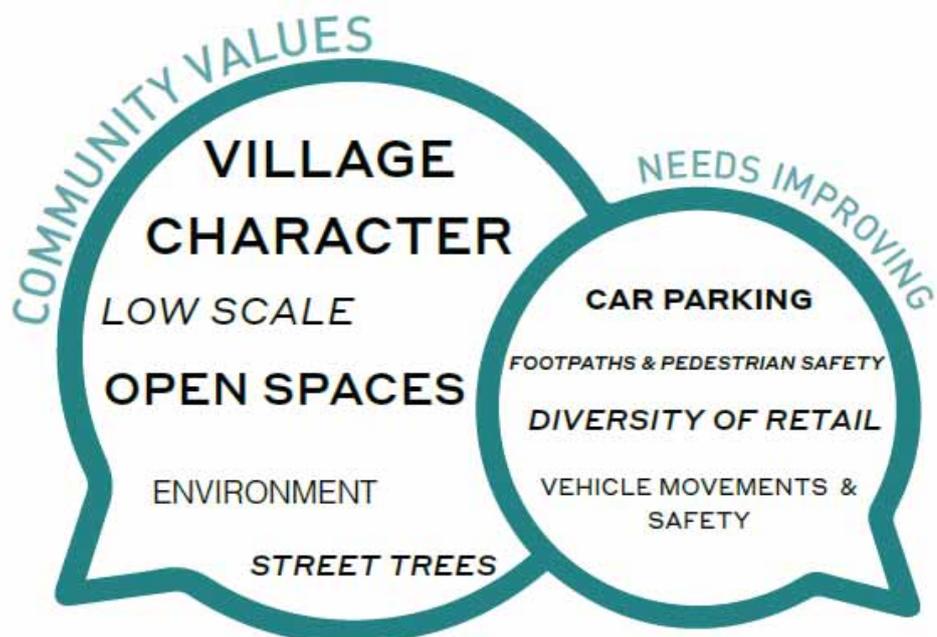
The role the surrounding environment plays in determining the image of the town is also a priority from consultation respondents, particularly the open spaces and Jacksons Creek escarpment. The need to protect existing street trees and continued planting of canopy trees in the town centre and surrounding open spaces has also been identified as a key issue.

Many respondents raise concerns in relation to car parking in the town centre including access, safety, the need for additional car parking spaces and additional temporary car parking spaces on market days.

Additionally, respondents highlight the need for a diversity of retail, particularly calling for more venues that operate in the evening. Respondents are in favour of providing local jobs in Gisborne, however many participants reveal a reluctance for the provision or appropriateness of 'big box' larger retailers in the town centre.

In general, the Gisborne community values the low scale, green village setting of the town centre, however feel it requires a more diverse offering of retail with improved provision for car parking.

The feedback received as part of these consultations has been considered, and where possible incorporated into the draft Urban Design Framework.



Further detail on engagement activities and responses is provided in the Gisborne Futures Engagement - Phase 1 & 2 Summary Report.

4. Town Centre Land Use

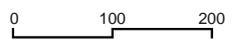
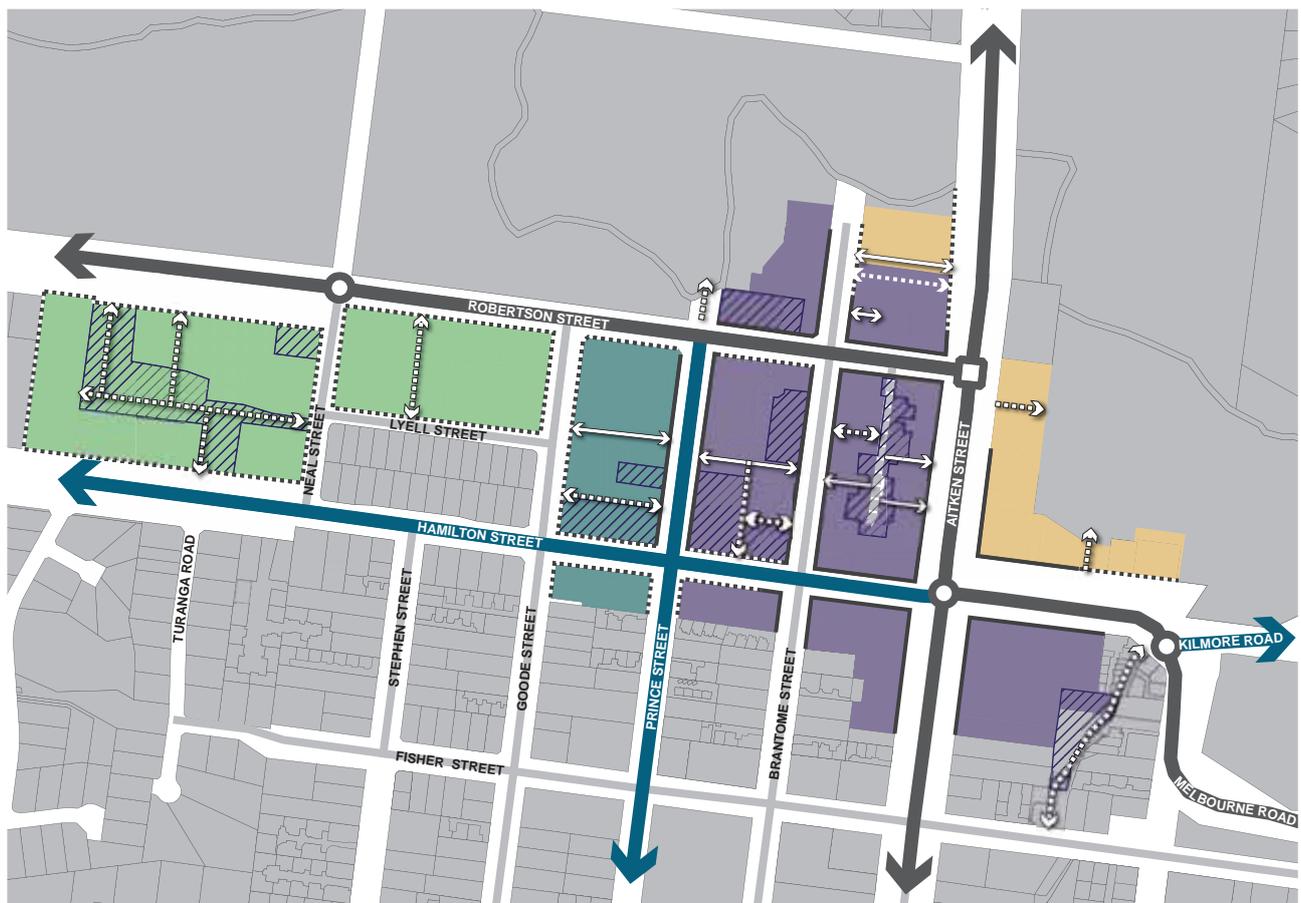
Strengthen the role of the town centre as Gisborne's hub for community facilities and activities and provide opportunities for local economic development in all sectors, that will comprise a mix of thriving retail, commercial, service and health uses.

The town centre is Gisborne's primary activity centre which provides anchor retail, commercial and community activities. Retail is the largest industry of employment in Gisborne and the town centre accommodates significant and diverse retail mix. Gisborne services a larger area than its direct township in the provision of retail goods and services, including Macedon and Riddles Creek.

To maintain the compact, walkable nature of the town centre future development will be accommodated within the existing commercial area.

There is currently 4000sqm of vacant land in the Commercial 1 Zone. Additional retail and commercial floorspace will require infill of existing vacant sites and through redevelopment of opportunity sites in the town centre. This could include use of commercial or residential development at upper levels, with ground levels primarily used for retail purposes.

There is potential for light industrial uses north of Robertson Street to relocate to future Commercial 2 or industrial land in the business park. This would provide opportunity for redevelopment of these sites for medium density, mixed use development appropriate to the town centre.



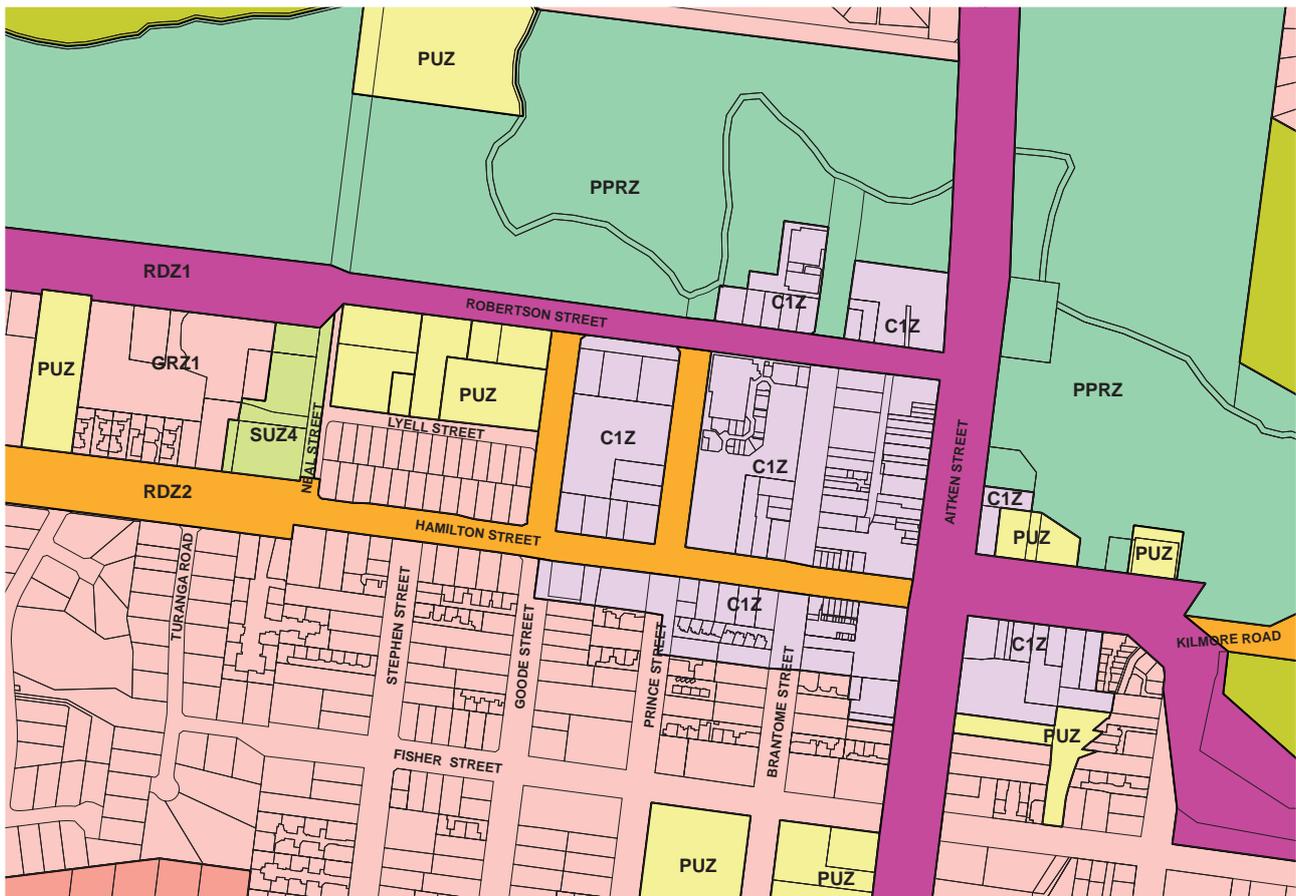
- Arterial Roads
- Connector Roads
- Local Roads
- Laneways
- Primary street frontages
- Secondary street frontages
- Existing pedestrian connections
- Potential pedestrian connections
- Retail Precinct
- Commercial Precinct
- Community Precinct
- Health & Civic Precinct
- Development opportunity sites

Figure 1. Town Centre Land Use Framework

4.1. Zoning

Currently, land within the Gisborne Town Centre UDF area is zoned:

- Commercial 1 Zone (C1Z) of which the primary purposes are to create vibrant mixed use commercial centres for retail, office, business, entertainment and community uses and to provide for residential uses at densities complementary to the role and scale of the commercial centre;
- Public Use Zone (PUZ) of which the primary purposes are to recognise public land use for public utility and community services and facilities and more specifically in the context of the Gisborne Town Centre to provide for service and utility, health and community, local government and other public uses;
- Special Use Zone (SUZ) which currently provides for uses related to a private hospital;
- Public Park and Recreation Zone which aims to recognise areas for public recreation and open space, to protect and conserve areas of significance where appropriate and provide for commercial uses where appropriate; and
- General Residential Zone (GRZ) of which the primary purposes in the context of a town centre are to encourage a diversity of housing types and housing growth particularly in locations offering good access to services and transport and to encourage development that respects the neighbourhood character of the area.



Existing Zones

GRZ1 General Residential Zone - Schedule 1	PUZ Public Use Zone
C1Z Commercial 1 Zone	SUZ Special Use Zone
PPRZ Public Park & Recreation Zone	RDZ1 Road Zone Category 1
	RDZ2 Road Zone Category 2

0 100 200

Figure 2. Existing Zones



4.2. Retail Precinct

The retail precinct will continue its focus as the core retail function of the town with a vibrant mix of convenience and specialty retail stores, restaurants and cafes that contribute to a thriving day and night economy.

The village feel of the precinct will be retained and enhanced through high quality architecture, street tree plantings, increased pedestrian amenity and public spaces that reflect the history and local character of Gisborne.

Food retail is Gisborne's strength, anchored by three supermarkets of differing offers (Coles, IGA and Foodworks) and complemented by specialty retail stores, cafes, take away shops and restaurants located within the town centre retail core. In addition to the traditional retailers, the monthly Gisborne Market is a feature of the retail offering of the town. The proposed ALDI supermarket at 47-51 Aitken Street will draw some custom and activity to the south of the retail core.

To the north of Robertson Street is a large pet supplies store and a strip of small factories that include panel beaters and various trade supplies outlets. The Gisborne Futures Structure Plan contains strategic preference to direct light industrial, trade supplies and restricted retail premises to an expanded business park on Saunders Road in New Gisborne.

The Gisborne Town Centre currently supports approximately 25,845 sqm of occupied retail floorspace. The current vacancy rate is 2%, indicating strong retail performance.

4.3. Mixed Use Development

Mixed use development is promoted across all vacant sites and key opportunity sites. There is potential for greater housing diversity to be provided through shop-top apartments above retail or commercial floor areas. Residential opportunities in the town centre will contribute to a greater level of activity in the evening, provide walkable access to amenities and increase surveillance and activation of built form.

4.4. Commercial Precinct

The commercial precinct will provide office and business development opportunities with active frontages along street walls and potential upper-storey residential development.

Pedestrian safety and amenity through central car parking areas will be improved by designated pathways, crossings and way-finding.

Offices and services are primarily located between Prince and Goode Street, focused on the Nexus Centre and its surrounds. There is approximately 18,265 sqm of occupied commercial floorspace in the Town Centre and over 1,100 sqm of vacant floorspace, resulting in a vacancy rate of 6%. The majority of floorspace is in office (13,256 sqm).

Future Retail and Commercial Floorspace Projections

Based on the population forecasts the combined retail and commercial floorspace projections for Gisborne equate to the need for approximately 18,000 to 19,000 sqm of additional floorspace by 2036.

To ensure the compact, walkable nature of the Town Centre is maintained, future retail and commercial development is to be focussed on available vacant sites and strategic redevelopment sites rather than expansion of the town centre commercial zoning. There is limited vacant land in the Commercial 1 Zone (0.4ha), meaning that future commercial and retail floorspace will need to be accommodated through multi-level development of these sites.

4.5. Civic and Health Precinct

Health and wellbeing outcomes will be promoted by focusing healthcare, medical and supporting services within the precinct, and by connecting these services to the surrounding reserves, gardens and town centre through a safe and accessible pedestrian network.

This precinct will provide for housing typologies for all life stages including aged care and opportunities for diverse housing set within a generous, landscaped environment.

The Robertson Street frontage will continue to accommodate civic services and the emergency services role will be recognised and maintained.

4.6. Community Precinct

The community precinct will be a focal point for community activities that builds on the strengths of existing facilities and maintains a strong connection to Gisborne’s network of open spaces and recreational reserves.

Physical and visual linkages from the precinct to the town centre will be enhanced through improved pedestrian connections, landscaping and public realm treatments. The precinct will provide vibrant community gathering spaces and opportunities for temporary or seasonal activities.

4.7. Land Use Direction

- Support development that accommodates a mix of retail, commercial, community and residential uses within the town centre.
- Focus future retail and commercial development on available vacant sites and strategic redevelopment sites rather than expansion of the town centre commercial zoning.
- Support location of light industrial, trade supplies, restricted retail and bulky goods to an expanded business park, and discourage their location within the town centre.
- Ensure that land uses are compatible and their establishment doesn’t compromise the amenity of existing or potential future activities in the surrounding area.
- Avoid single use development of vacant or potential development sites.
- Ensure any new large format development provides streetscape activation through sleeving with active uses and sensitive location of servicing, vehicle access and loading facilities.

Preferred Land Use

Table 1 illustrates preferred land use activities within the nominated precincts. There is already a broad mix of uses within the town centre and land use preferences generally reinforce the predominant activity that is occurring. Some discretion and a flexible approach will be required to ensure market demand and viability of development on sites is supported.

Table 1. Preferred Land Use

Retail Precinct	Commercial Precinct	Civic & Health Precinct	Community Precinct
<ul style="list-style-type: none"> • Ground floor retail with upper level residential or office • Supermarkets • Discount department stores • Specialty retail • Showrooms • Shops • Cafes • Restaurants • Bars and Clubs 	<ul style="list-style-type: none"> • Offices • Ground floor office with upper level residential • Local service industry • Child care • Health and beauty services • Education 	<ul style="list-style-type: none"> • Medical services • Aged Care • Retirement Living • Child care • Private and independent education facilities • Emergency services • Place of assembly 	<ul style="list-style-type: none"> • Council facilities (library, indoor sports centre, community centre) • Health facilities • Community services

5. Entrances & Gateways



Promote Gisborne as a ‘village in the valley’ through retention and enhancement of township edges and entrances.

Gisborne’s town centre is contained within the Jacksons Creek valley. The topographical change, wide streets lined by mature deciduous trees and mountain views to the north and south are all important elements that contribute to Gisborne’s unique sense of place.

The township entrances and edges provide a series of very different and unique landscape experiences. Main roads from the surrounding region converge on the town centre of Gisborne. They come from the heavily forested entrance from Bullengarook, to the undulating hills and bush in the south, the mountain terrain in the north and the rural farmland in the east. From each direction there is a clear transition into Gisborne’s valley at Jacksons Creek, creating a sense of arrival into the town centre.

The character of these entrance roads is influenced by accumulation of public and private realm elements such as built form, wide road reserves, avenue trees, landscaping and views and vistas to surrounding landscape features.

Entrances mark the transition from open rural landscapes to the township, and provide a visual clue to the identity and character of the town. The elements that form the significant landscape in these gateways are closely associated with the rural and country character of Gisborne.

Entrances are vulnerable to features that detract from the arrival experience including excessive signage on both public and private land, overhead power cables (and associated tree lopping), inconsistent landscape treatments and sound walls. Edges are subject to peripheral development businesses seek to gain exposure to passing traffic with signage, billboards and insensitive or branded built form.

5.1. Gateway Guidelines

- Strengthen sense of place and arrival in the Gisborne town centre with buildings of exemplary architectural quality on prominent sites and in locations adjacent to existing and proposed public spaces.
- Building materials are to respond to the semi-rural character of the town, and buildings wholly coloured in branded colours or text are avoided.
- Ensure any new development and streetscape works maintain and enhance avenue tree planting along township entrance roads.
- Apply landscaped setbacks at the edges of the town centre to transition to a more compact urban space.
- Prepare a signage strategy or local policy for signs and advertising that ensures the size and height of signs are compatible with the scale of the building or site on which they are displayed, the surrounding streetscape or landscape and the size and nature of other signs in the area.



- Primary Entrance Road
- Secondary Entrance Road
- Key township gateways
- ★ Prominent corner and gateway sites

Figure 3. Entrances & Gateways

6. Town Centre

Built Form

Retain and enhance the village qualities of Gisborne's town centre by encouraging excellence in architectural and urban design that responds to the character, history and identity of Gisborne.

Enhance the pedestrian experience and encourage activity, economic prosperity and social interaction within the town centre.

6.1. Existing Conditions

Gisborne's town centre has a generally low-scale built form character that features a variety of architectural styles.

Most buildings are simple in design with heights of one or two storeys and built to the edge of front and side boundaries. This has created a continuous street wall and attractive pedestrian streetscape, particularly along Aitken Street and the eastern side of Brantome Street between Robertson and Hamilton Streets. These are generally fine-grained with narrow shop fronts, verandahs and awnings that offer weather protection and have outdoor dining or trading that activates the street.

The Nexus Centre is the only three storey building and is a stand-alone development with a number of adjoining vacant development sites.

North of Robertson Street is some light industrial development that features low scale factories with large front setbacks dominated by car parking.

The Gisborne Village Shopping Centre is a solid building, with shop fronts facing the two large at-grade car parks. Externally, some shops have active frontages although there are large blank walls along Prince Street and Robertson Street. Loading bays and bin storage on external walls of the shopping centre present a poor interface with the public realm, however the addition of a mural by artist Julian Clavijo has brightened the northern façade

Heritage Way provides back-of-house access to the shops fronting Brantome and Aitken Streets. Underutilised spaces present an opportunity for greater built form density and the development of a compact, activated laneway environment.

6.2. Built Form Considerations

Currently Gisborne does not have any enforceable built form controls or height limits. The 2008 UDF provides some direction however was never formally implemented through the Macedon Ranges Planning Scheme and therefore only has limited statutory weight in decision-making.

The quality and design of buildings and streetscapes in the town centre is important to the overall image and appearance of Gisborne making it an attractive place to live and to invest, which in turn contributes significantly to the economic viability of the township.

Managing building scale and the relationship between new and old development is key to retaining the village character of the Gisborne.

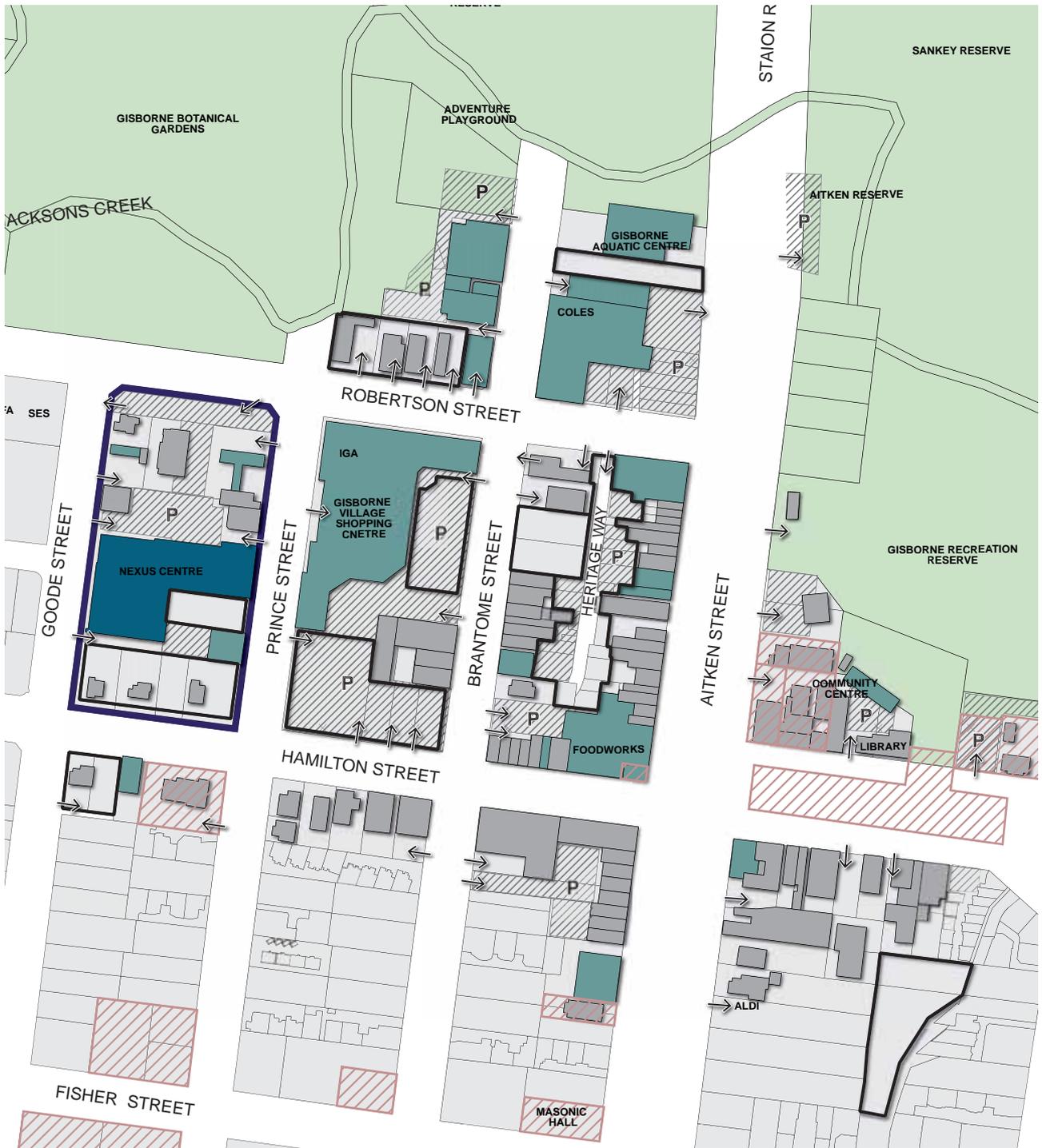
A number of under-utilised sites and buildings detract from the vibrancy of the town centre. Blank walls, unsympathetic cladding and excessive advertising signage compromises the quality of streetscapes. Many sites have capacity for greater height and building mass, therefore clear guidance on managing transitions in height and form are required.

The Gisborne Futures Structure Plan seeks to consolidate the town centre and accommodate future retail, commercial and residential floorspace within the existing town centre boundaries. To achieve this, the potential of existing development sites must be maximised to provide a mix of uses.

There is a need to increase the number of dwellings to meet the expected population growth. The Gisborne town centre has the capacity to accommodate apartments within mixed-use developments that contribute to the provision of a diversity of housing options, and designed to have good internal amenity, facilities and services.

Built form controls have been developed to provide direction for future development to contribute to an active and vibrant town centre. They are not prepared to restrict development. They seek to achieve a high quality built form and streetscape outcome while maximising the economic and commercial potential of sites.

Built form controls are to be implemented through a Design and Development Overlay.



Existing Built Form: Town Centre

- | | |
|---------------------------|------------------------------------|
| 1 storey building (5m) | At-grade car parks |
| 2 storey building (5-10m) | Development opportunity sites |
| 3 storey building (10m+) | Vehicle crossovers & access points |
| Heritage Overlay | Parks & reserves |
| Development Plan Overlay | |



Figure 4. Existing Built Form: Town Centre

6.3. Built Form Objectives

- Retain and enhance the village qualities of Gisborne’s town centre by supporting a fine grain of development, and encouraging excellence in architectural and urban design that responds to the character, history and identity of Gisborne.
- To recognise the Gisborne town centre as a focus for commercial, retail and residential growth, and implement design controls to guide development to respond to the preferred future township character.
- To maximise the potential of development sites to provide a mix of uses, including commercial and retail spaces at ground floor, with residential or office spaces at upper levels.
- To encourage medium-rise, mixed use development of three to four storeys, setback behind the established two storey street wall.
- To discourage underdevelopment of sites to ensure accommodation of future demand for retail, commercial and residential space within the existing town centre boundary.
- To strengthen the sense of place and arrival in the Gisborne town centre with buildings of exemplary architectural quality on prominent sites, entrance roads and locations adjacent to existing and proposed public spaces.
- To provide guidance for new development to respond to the existing character of the street as set by the scale and form of existing buildings, topography, streetscape pattern, trees and open spaces.
- To ensure that development provides active, fine-grain shop fronts through appropriate building rhythm, proportions and detailing, particularly on wider sites.
- To ensure development provides a suitable design response to sensitive interfaces, including residential properties and open spaces.
- To ensure new development, streetscape and road work maintains and enhances key aspects of township character and historical streetscapes, particularly significant trees with high retention value within roadsides, streets and open space reserves.
- For new development to maintain or provide active laneways, connections and arcades to reinforce a permeable and connected pedestrian network throughout the town centre.

6.4. Building Height

The overall height of any new building must not exceed the maximum height limit and must meet built form objectives. Preferred building heights are illustrated in Figures 5 and 6.

Two to three storey (13m) overall building heights are preferred. Four storeys will only be considered on consolidated or larger sites where:

- Height is proportional to the building footprint.
- It can be demonstrated that upper levels are recessive as viewed from the street and any adjacent sensitive interfaces.

The maximum height limit cannot be varied with a permit. The maximum building height does not include:

- Lifts, stairs, rooftop plant, solar panels designed to the satisfaction of the responsible authority.
- Structures and equipment associated with communal or green roof areas that occupy 50% or less of the roof area, and are designed to minimise views to any adjacent property.

On narrow individual lots, application of upper storey side setbacks above the street wall height may not be achievable (refer section 5.7 on page 24), therefore a building height of 8.5m is preferred.

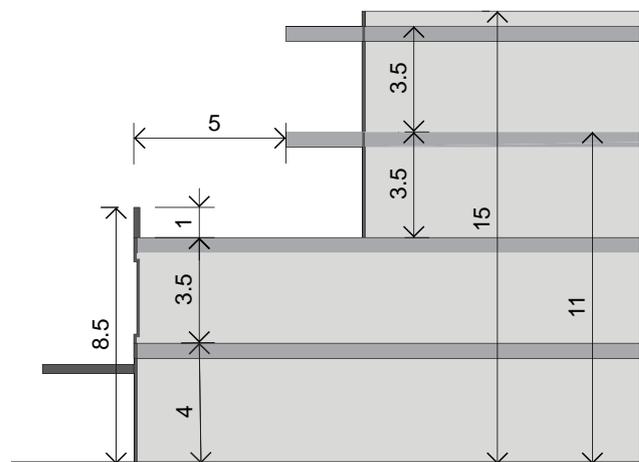


Figure 5. Building Heights and Setbacks



Figure 6. Preferred Building Heights and Setbacks: Town Centre





6.5. Street Wall

The predominate street wall of Gisborne is one to two storeys which provides a low-scale character of development across the town centre as viewed from the street. The Nexus Centre is the only exception to this. A continuous street wall height will ensure that new development respects and responds to this predominant character, and retains a ‘human scale’ pattern of development that does not overwhelm the street.

- The street wall of any development within the town centre must not exceed 8.5m, or the height of adjacent street wall, whichever is greater.
- The street wall height applies to all buildings to ensure the general low scale form is maintained at the street edge, while still allowing a reasonable scale of development to occur beyond this. This applies to buildings with zero front setbacks and landscaped front setbacks.
- Street wall heights allow for 4m ground floor levels, to accommodate adaptive uses, and 3.5m upper levels, with parapets or other features that provide articulation to frontages (refer Figure 5 on page 20).
- Street wall heights are applicable to laneways and pedestrian thoroughfares.
- The maximum street wall height limit cannot be varied with a permit.

6.6. Building Frontages

Primary Frontages

Buildings with Primary Frontages will have a pedestrian focus with a fine grain of development, active building frontages, weather protection and a high quality public realm with wide footpaths, street trees, landscaping and furniture.

- Additional vehicle crossovers and car park entries should be avoided along these streets to minimise pedestrian conflicts and provide a continuous street wall. Coordinated development of land should allow for rear lane vehicle access, or shared access with existing developments should be negotiated, where possible.
- Buildings are required to be constructed to front and side property boundaries to maintain a consistent street wall.
- Provide wide pavements (minimum 5m, with 2m ‘pedestrian zone’) in front of any new commercial or retail development to cater for high pedestrian flows, outdoor dining and other street activity.

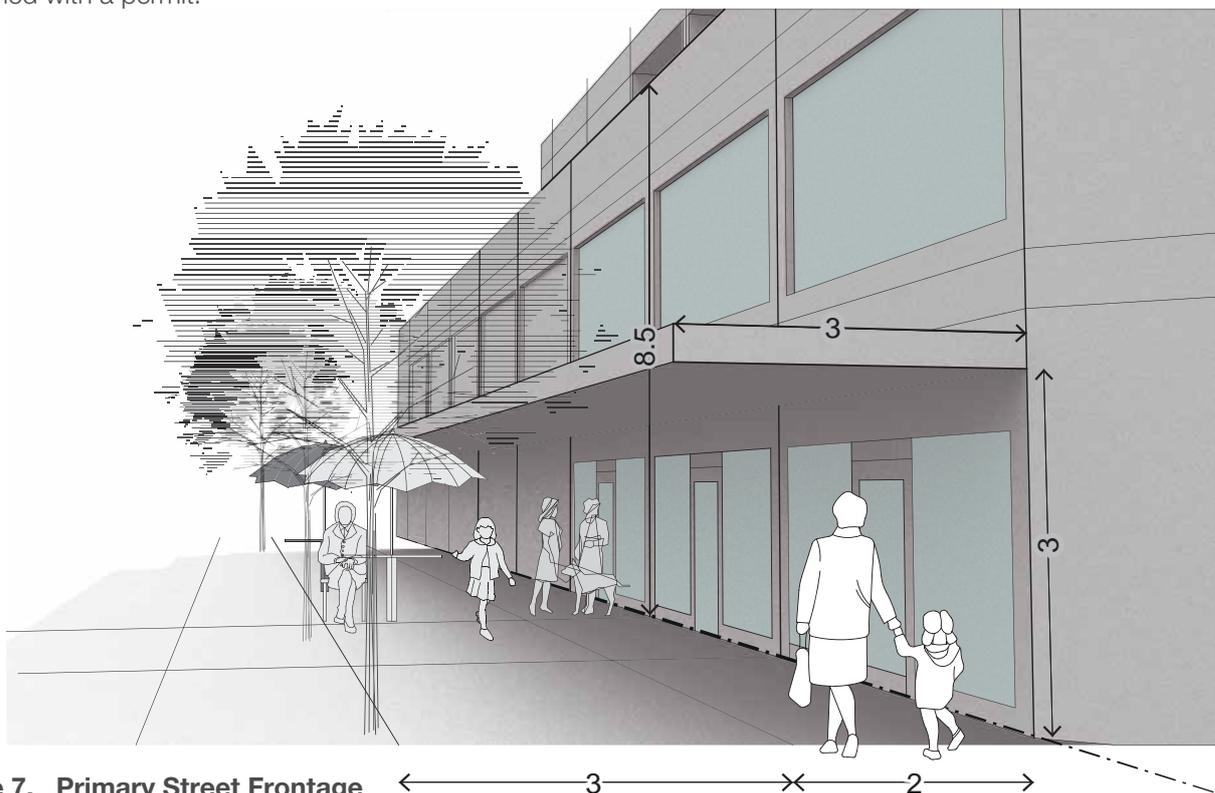


Figure 7. Primary Street Frontage

Secondary Frontages

Streets with Secondary Frontages accommodate a mix of active frontages with buildings set into landscaped surrounds.

- Some loading, car park or service access may be permissible where these cannot be provided via laneways or otherwise avoided. Ensure that no frontage is completely dominated by servicing or car parking.
- A 5m landscaped front setback is required on some secondary streets where there is a transition into the town centre from residential areas.
- A landscaped front setback is applicable to sites where higher built form may be permissible to allow ample space for canopy tree planting to reduce the visibility, scale and bulk of higher built form as viewed from the street.
- Landscape setbacks apply to sites located opposite or adjacent to areas that are expected to maintain a lower scale of development, such as residential areas or commercial buildings of 1-2 storeys, to park interfaces and to provide a transition to the denser form of the retail core.
- Landscaped front setbacks are to allow for landscaping, canopy tree planting and pedestrian entries.
- Vehicle access for multiple sites should be consolidated to avoid additional crossovers on the street.

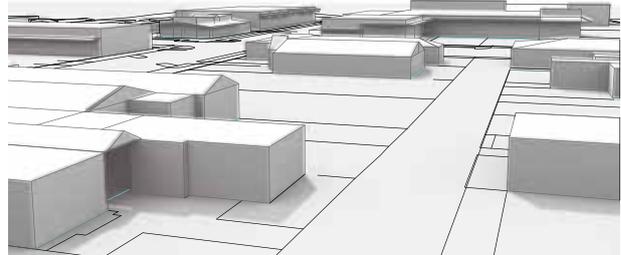


Figure 8. Secondary street frontage with 5m landscape front setback

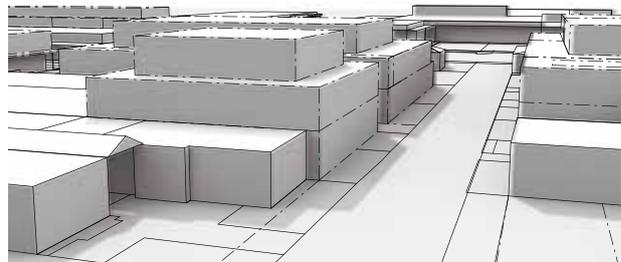
6.7. Upper Level & Side Setbacks

Upper level setbacks minimise the visual bulk of new development, and allow for active uses such as terraces and balconies to be accommodated behind the street wall. Side setbacks allow for light access to streets to streets and laneways, ensure that adjacent sites are not overwhelmed by new development, and that their future development potential is not compromised.

- Upper level setbacks of 5m above the street wall on the frontage of all buildings are required across the town centre.
- 5m upper level side setbacks are to be applied to sites adjacent to existing development.
- Upper-level setbacks must allow for the development of adjacent upper levels, and to provide light access to future built form.
- Development must provide quality treatments to interfaces that will remain undeveloped for the short to medium term.
- New development adjacent to any heritage building must provide setbacks that ensure that views to heritage features, including side walls and facades, are retained.



Heritage Way: existing built form.

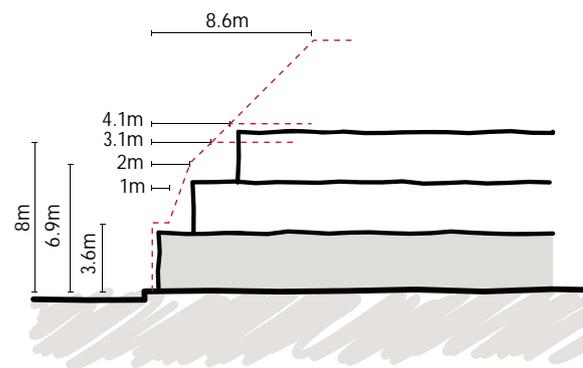


Heritage Way: Potential built form envelopes with 5m upper level setbacks applied.

6.8. Sensitive Interfaces

New development is to be designed to minimise impact on adjacent sensitive uses including community facilities, areas of open space and existing residential areas. Design measures to reduce impact on sensitive interfaces include:

- Provide a transitional scale of building heights and side and rear setbacks along sensitive interfaces in accordance with residential requirements (Clause 55 Standard B17).
- Considered location of plant and service areas for visual and acoustic amenity.
- Provide site-specific design responses to address areas of sensitivity.



Side and Rear Setbacks



6.9. Active Frontages

An appealing and well-designed frontage to a business will attract patronage and engage passing pedestrians. Good visual merchandising can make an area feel active and welcoming. Blank walls, high fences and windows covered with posters, signage and cluttered displays result in poor visual connections which inhibit casual surveillance and contribute to poor perceptions of safety and a closed feel to the street.

Opportunities for active frontages should be maximised on all primary and secondary streets in Gisborne, through the following guidelines:

- Locate building entries so that they are orientated to the street.
- On corner sites buildings are to address both street frontages.
- Provide active ground floor uses at street level with clear glazing for at least 75% of the ground floor street frontage, including on buildings such as supermarkets that may desire external blank walls.
- Provide balconies and terraces at upper levels to allow visual connection to the street and increase opportunities for passive surveillance.
- Ensure buildings fronting areas of open space provide active edges and/or habitable rooms and pedestrian access.
- Promote on-street trading and outdoor dining.
- Good visual connections between the shops and the street through visual merchandising, minimal signage and creative internal lighting are encouraged.



6.10. Laneways & Vehicle Access

Avoid additional car parking, servicing and vehicle access via primary street frontages including Prince Street, Brantome Street, Robertson Street and Aitken Street. In these areas, retention and development of active frontages and pedestrian amenity is prioritised.

- Locate car parking access points to the side or rear of sites where possible, utilising a side or rear street or laneway. On larger development sites, laneways or 'back of house' access should be negotiated through existing access points. Application of access easements may be necessary to achieve this.
- Laneways are to be a minimum of 7.5m to allow for shared pedestrian and vehicle access, and to provide spaces for active edges.
- Laneways should be treated as 'shared zones' that provide pedestrian thoroughfares as well as providing service and car parking access to adjoining properties.
- Businesses are encouraged to provide active frontages to laneways, or have secondary entrances that provide safety and surveillance.
- An attractive laneway may provide address for uses in upper levels of buildings without impacting on the retail or commercial street frontages.
- Upper level setbacks of 5m are required above the 8.5m street wall to ensure laneways have light access and to achieve a separation between buildings
- In locations where access to car parking from the street frontage is unavoidable, limit the width of the access point to a maximum of 6 metres and no more than a third of the overall site frontage width.

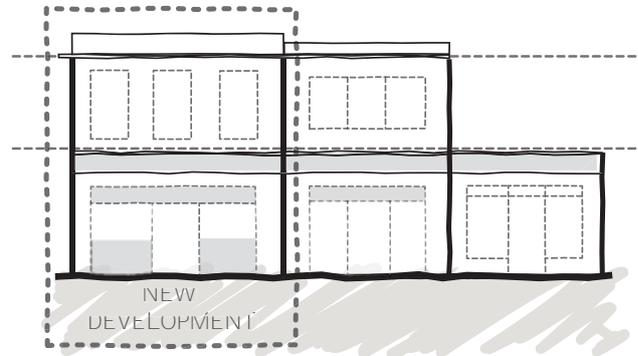
6.11. Building Massing and Articulation

A 'fine grain' pattern of development includes narrow shop fronts that house a variety of local businesses and built form scale that does not visually overwhelm the streetscape, or dominate the landscape or topographical context.

Building massing diagrams are a 'starting point' for the design of new buildings within the town centre. These illustrate the general building envelopes guided by height and setback parameters for potential development.

Articulation to these is required to further break up the building mass, and to provide a specific design response that is sensitive to existing buildings and the development potential of adjacent properties.

- Design of individual buildings within the prescribed envelopes is to divide the form so that it reads as a series of discrete elements. This will reduce the scale and bulk of built form, create visual interest and provide amenity to future occupants.
- Retain and enhance the fine grain pattern of shop fronts. A vertical rhythm should be introduced to break up the facades of buildings with a wide street frontages. This may be through including a range of parapet heights, windows and openings, recesses or other architectural detailing.



- The size of new buildings should be responsive to the surrounding streetscape and built form character, and buildings should be articulated to maintain the fine grained pattern of development.
- Large 'box-like' buildings that provide minimal articulation and detract from the low scale urban form of the area are to be avoided.
- New buildings are to respond to topography and township character, and be designed so that key views are not obstructed.
- Building heights and form are also to consider the impacts on views towards the site from nearby vantage points.
- Design taller buildings to limit overshadowing of public spaces, and to provide sun penetration along key pedestrian routes and into public spaces and community areas.

6.12. Architectural Treatment

Development should maintain and enhance Gisborne's township character though considered and sensitive use of materials and finishes.

- Contemporary architecture that complements the character of existing buildings through form, scale and material choice is encouraged.
- Minimise the visual impact of new development through use of simple material palettes with muted colours.
- Use high quality materials that do not generate glare and can withstand the effects of weathering such as brick, painted brickwork, timber and transparent glass for the main body of a building.
- Render and metal cladding should only be used as secondary or highlight finishes.
- Materials should be selected to provide an appropriate level of detailing and visual interest without creating a cluttered appearance on the façade.
- Building features and details should be highlighted and enhanced through the use of materials (for example, metal and timber sun-shading devices and architectural features).
- Avoid mock-heritage styles of architecture.
- Integrate roof forms with the overall building façade design and on larger buildings, articulate or divide roof forms into distinct sections to minimise visual bulk.
- Frame important corners for wayfinding and visual legibility by ensuring built form addresses both street corners, and by emphasising building form at key corners and entry points for the town centre.
- The visual impact of new development is to be minimised through use of simple material palettes with muted colours.

.....

6.13. Signage and Branding

Signage should be designed to complement, rather than dominate or intrude upon, the character and visual amenity of an area, the buildings on which they are displayed, and the general environment.

- A focussed signage strategy or local policy for signs and advertising should be prepared to ensure that the size and height of signs are compatible with the scale of the building or site on which they are displayed, the surrounding streetscape or landscape and the size and nature of other signs in the area.
- Signage is to be limited to business identification and incorporated into the design of buildings.
- Signage is to be minimised to prevent visual clutter particularly along township gateways and entrances, and within road reservations and commercial/retail areas.
- Signage and branding should comprise no more than 8m² or 10% of a building frontage.
- Third-party advertising or billboards are not permitted in Gisborne town centre or along entrance roads.
- Buildings wholly coloured in branding colours or text are not permitted.

6.14. Lighting

Perceptions of safety are diminished at night when traffic and pedestrian levels are low, more businesses are closed and general light levels are reduced. While two or more shops may be open into the evening, closed businesses between them may leave dark and uninviting spaces. New developments should consider creative lighting solutions inclusive of:

- External illumination of building entries and façades.
- Consistent lighting under awnings;
- Decorative lighting of open space areas, pedestrian connections and outdoor seating areas.
- Illumination of trees and landscaping.
- LED lights embedded in pavement and street furniture.

To enhance the night-time environment Council should consider undertaking a lighting assessment and preparing a Lighting Strategy for the centre. Priority should be given to improving pedestrian walking routes to key destinations.

6.15. Services

- Services including electrical substations and fire service equipment should be located on laneways or away from street frontages wherever possible. These are to be integrated into façade design to minimise their impact on the public realm.
- Locate roof services such as air conditioners, plant equipment and lift overruns so they are not visible from the street or adjoining buildings. Screening of these elements should be incorporated into the design of the building.

6.16. Key Development Sites

To maintain the compact, walkable nature of the town centre future development will be accommodated within the existing commercial area.

There is currently 4000sqm of vacant land in the Commercial 1 Zone. Additional retail and commercial floorspace will require infill of existing vacant sites and through redevelopment of opportunity sites in the town centre. This could include use of commercial or residential development at upper levels, with ground levels primarily used for retail purposes.

There is potential for light industrial uses north of Robertson Street to relocate to future Commercial 2 or industrial land in the business park. This would provide opportunity for redevelopment of these sites for medium density, mixed use development appropriate to the town centre.

The town centre contains a number of sites that are either currently vacant or have future redevelopment potential. These include:

1. Vacant land at 27 Brantome Street.
2. 49 Hamilton Street
3. 44 to 50 Hamilton Street
4. 16 Prince Street
5. Vacant and underutilised land within the health/ civic precinct
6. SUZ land adjacent to Bunjil Creek

Sites that have redevelopment potential in the longer term include:

7. Land occupied by light industrial and commercial uses at 14 to 20 Robertson Street
8. Rear of sites fronting Heritage Way.
9. At-grade car parks within the Gisborne Village Shopping Centre.

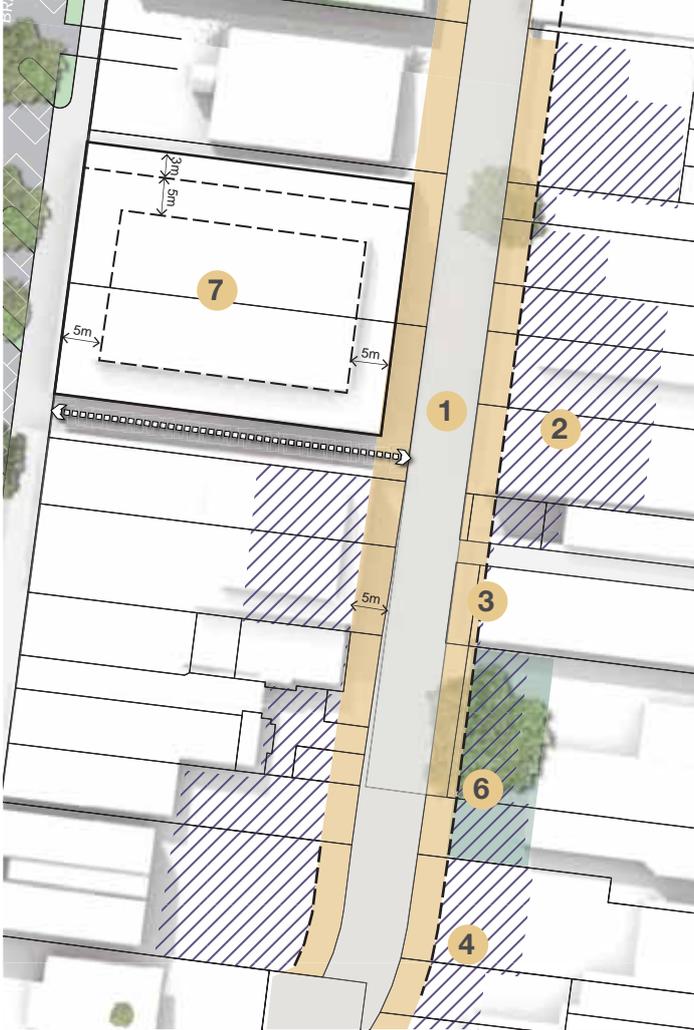
6.17. Development Site Concept Plans

Development site concept plans have been prepared to visualise how preferred built form and streetscape outcomes may be achieved on some of the larger sites within the town centre.



Figure 9. Development Opportunity Sites

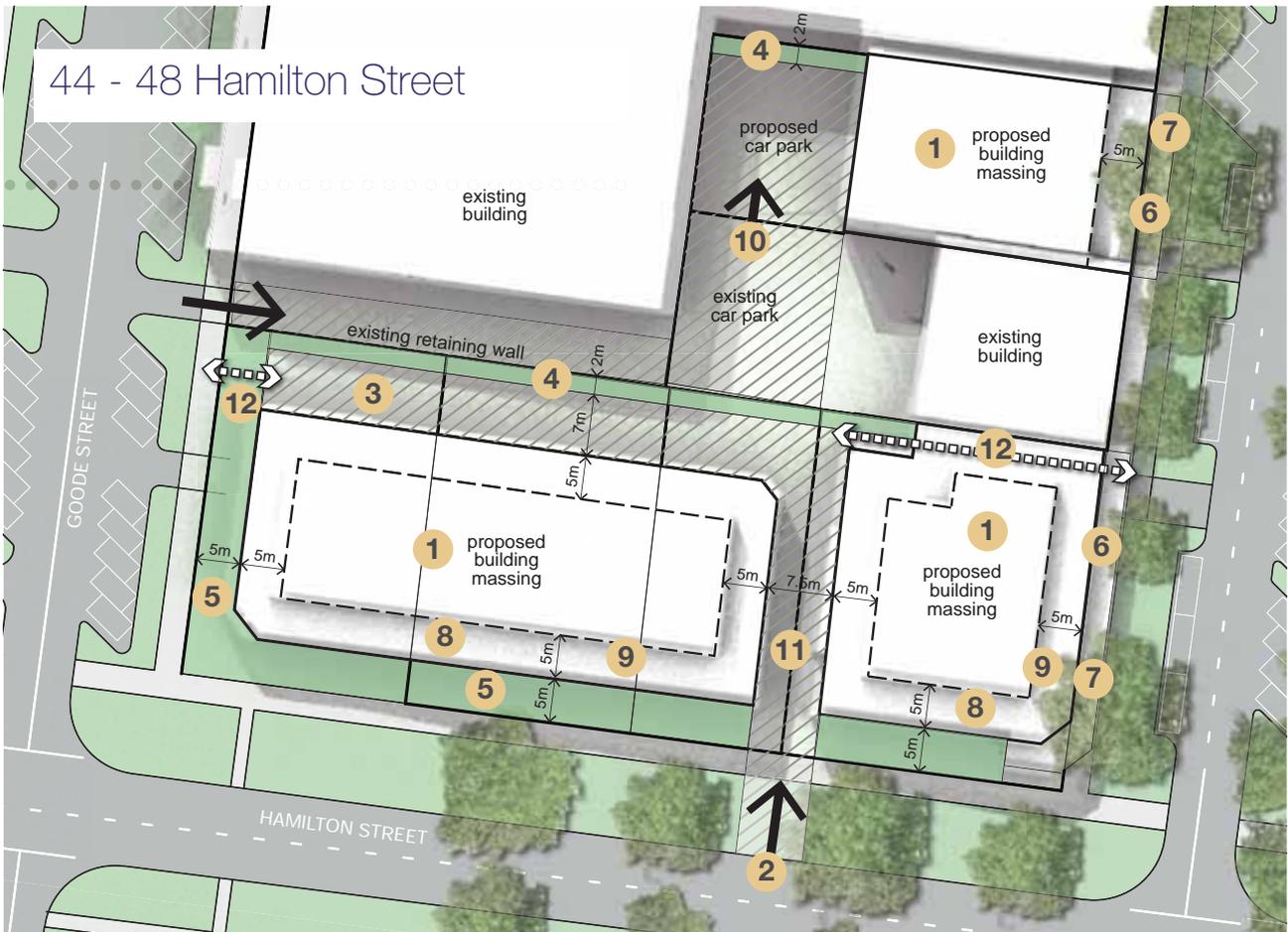
Heritage Way Concept Plan



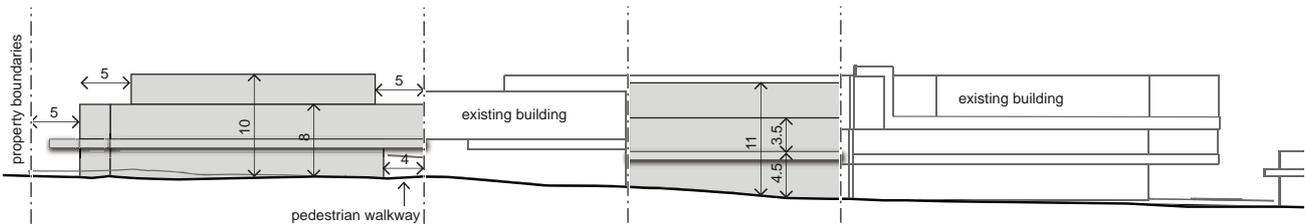
- 1 Heritage Way has the potential to develop as an activated laneway and shared user zone designed as an attractive and comfortable pedestrian environment that still allows loading, service and car parking access.
- 2 The space behind the existing shops has the potential to accommodate a greater density of development. An attractive laneway may provide address for uses in upper levels of buildings without impacting on the retail frontages to Aitken and Brantome Streets.
- 3 Businesses are encouraged to provide active frontages or have secondary entrances which provide safety and surveillance.
- 4 Spaces for bins and other storage are to be appropriately screened to ensure a tidy presentation onto laneways and parking areas. Ensure that no frontage is completely dominated by servicing or car parking.
- 5 8.5m street wall applies to the laneway interface, with 5m upper level setbacks above the first floor.
- 6 Retain tree and open space at the back of 32 and 34A Aitken Street as a small pocket park / plaza.
- 7 Development at 27 Brantome Street is to:
 - Provide transitional setbacks to the church to the north.
 - Provide a public pedestrian accessway through the site.
 - Apply zero setbacks to Brantome Street, and maintain a 8.5m street wall height.



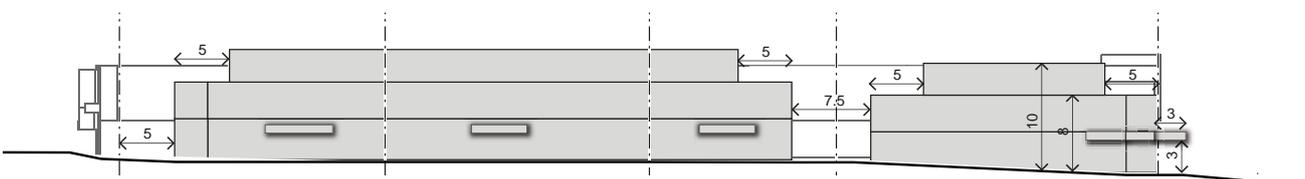
44 - 48 Hamilton Street



- 1** Mixed use development with ground floor retail/office spaces, with office and/or residential uses in upper levels.
- 2** Vehicle access to be consolidated to a single crossover located on Hamilton Street.
- 3** Attractive and safe shared user laneway to provide secondary building entries and pedestrian access, in addition to vehicle and service access.
- 4** 2m landscape edge with screening vegetation and opportunities for canopy trees to mitigate views into laneway and adjacent car park areas, and to screen blank walls.
- 5** 5m landscape setback to Goode and Hamilton Streets featuring building frontages and pedestrian entries to provide transition into town centre.
- 6** Pedestrian street edge to primary street frontage on Prince Street with zero front and side setbacks, active ground floor frontages and weather protection.
- 7** Provide wide pavements (minimum 5m, with 2m 'pedestrian zone') in front of new commercial or retail development to cater for high pedestrian flows, outdoor dining and other street activity.
- 8** Upper level setback above first floor of 5m to ensure these are recessive, as viewed from the street.
- 9** Provide balconies, terraces and active uses within upper level setbacks to allow for visual surveillance and maximise outlook.
- 10** Rear access to XX Prince Street to be negotiated through existing rear lane access/car park areas.
- 11** Activated, shared user laneway.
- 12** Pedestrian accessway.

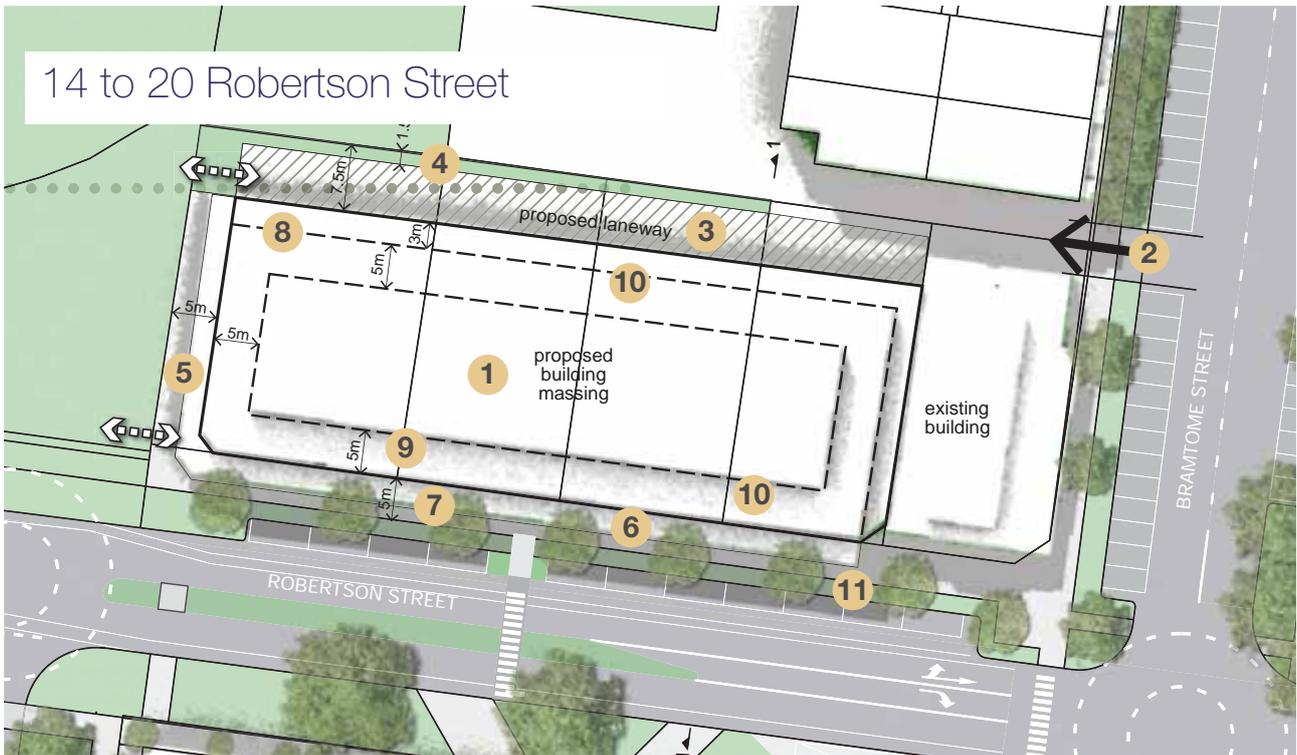


WEST ELEVATION

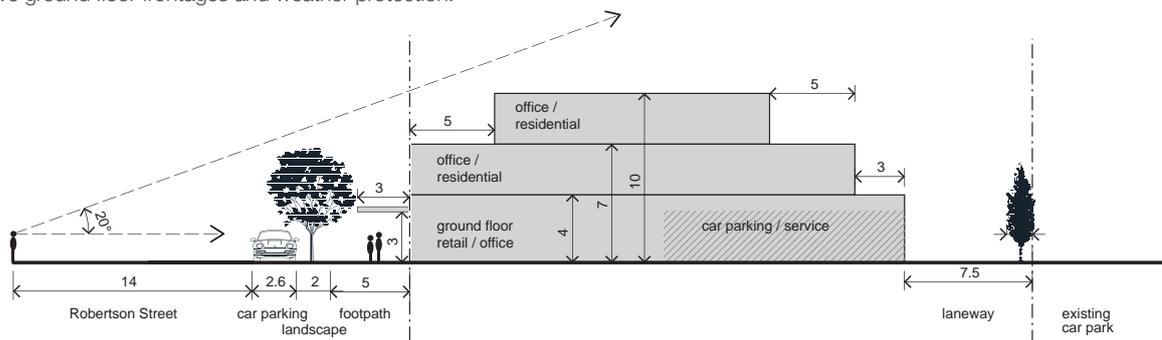


SOUTH ELEVATION

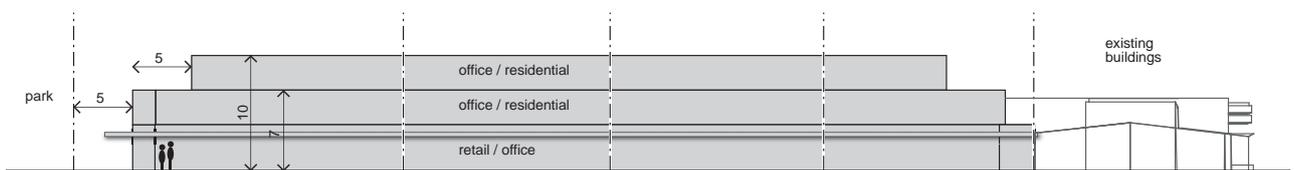
14 to 20 Robertson Street



- 1** Mixed use development with ground floor retail/office spaces, with office and/or residential uses in upper levels.
- 2** Vehicle access to be provided from single crossover located on Brantome Street.
- 3** Attractive and safe shared user laneway to provide secondary building entries and pedestrian access, in addition to vehicle and service access.
- 4** Landscape edge (min. 1.5m) with screening vegetation and opportunities for canopy trees to mitigate views into laneway, and visually screen higher built form as viewed from parkland.
- 5** Active built form edge to include landscaping, visual and physical connections to adjacent parkland.
- 6** Pedestrian street edge to primary street frontage (Robertson Street) with zero front and side setbacks, active ground floor frontages and weather protection.
- 7** Provide wide pavements (minimum 5m, with 2m 'pedestrian zone') in front of any new commercial or retail development to cater for high pedestrian flows, outdoor dining and other street activity.
- 8** Staggered upper level setbacks of 3-5m required to reduce visual bulk of built form, as viewed from parkland (refer Section 1).
- 9** Upper level setback above first floor of 5m to ensure these are recessive, as viewed from the street.
- 10** Provide balconies, terraces and active uses within upper level setbacks to allow for visual surveillance and maximise outlook.
- 11** Provide 2m landscape edge between car parking and pedestrian pavement to allow for canopy trees to enhance boulevard effect along Robertson Street.



SECTION 1



SOUTH ELEVATION

Gisborne Village Car Park Concept Plan

Large areas of surface car park have capacity to accommodate a greater mix of uses. They are relatively unconstrained by existing buildings and have existing access points that are clearly defined.

In their current form, surface car parks create gaps in streetscapes and reduce the amenity and appearance of the town centre. They create a number of conflicts and safety issues between pedestrians and vehicles. The provision of parking will remain a priority, however there is opportunity for this to be consolidated above and/or below ground in multi-deck structures that are sleeved by active uses such as office, retail or residential.

Flexibility in height controls may be considered to support the viability of sites, provided a high standard of built form and urban design quality is achieved.

The concept plan for the Gisborne Village Shopping Centre car parks explores a potential scenario for how these surface car parks may be developed for more intensive purposes, with consolidated vehicle access and an improved pedestrian environment.

Detailed design of car park sites in the long-term will require further consultation with land owners, traders and the broader community. It is proposed that a Development Plan Overlay be applied to this precinct to ensure that the future development of this land occurs in a coordinated manner.



- 1** Provide a 5m landscaped setback to Hamilton Street.
- 2** Provide a zero setback with active frontage to Prince and Brantome Streets.
- 3** 8.5m street wall applies across the site, with 5m upper level setbacks above the first floor.
- 4** Provide a public plaza or open space that promotes east-west pedestrian links from Brantome Street through to Prince Street.
- 5** Consolidate vehicle access and limit to existing crossover points. Vehicle access to any new undercroft or multi-storey car parking must be provided from internal access roads with no additional crossovers to be created along primary or secondary street edges.
- 6** Enhance pedestrian priority at vehicle entry points, pedestrians have right of way and this should be reinforced through visual and physical streetscape treatments.
- 7** Any new built form is to provide pedestrian streetscape treatments on all edges, including weather protection, opportunities for tree planting, kerb outstands street furniture and raised thresholds.
- 8** Consider under-grounding powerlines along Brantome Street between Robertson Street and Hamilton Street and enhancing tree planting.
- 9** Provide opportunities for pedestrian access.

Ensure car park edges provide a positive and engaging interface to pedestrian areas, including landscape treatment where necessary.

Bunjil Creek North

There is underutilised land central to the block bound by Aitken Street, Hamilton Road, Melbourne Road and Fisher Street.

There are a number of site features to note:

- Council-owned land once used as a depot and requires auditing from EPA to investigate potential contamination.
- Precinct is within Melbourne Water's 1:100 year flood mapping.
- Bunjil Creek has aboriginal cultural sensitivity.
- Bluestone channel is a heritage feature to be retained and improved.
- Land at 8 Melbourne Road has dual zoning, residential to the east and commercial to the west, and access is constrained by the creek.
- There are a number of large parcels with capacity for residential infill.
- Access to the internal area is currently limited to Fisher Street.

Future planning for this area is to carefully consider these constraints to ensure that internal areas aren't land-locked. The site has capacity for built form development in tandem with environmental and open space works to create an attractive, active interface to Bunjil Creek.

Design Requirements

A Development Plan Overlay (DPO) is proposed to ensure that future development is coordinated in This is to be prepared in consultation with all affected landowners, and is to consider:

- Providing a pedestrian and environmental open space connection along Bunjil Creek.
- Possible future access to the site through potential redevelopment of residential lots on Aitken Street.
- Access to the site from Fisher Street, which may require beaching or modification of the creek bank.
- Rear-loaded built form that presents an active and positive interface to the creek environment.
- Site specific height and built form controls, and response to site constraints.
- Open space and environmental restoration works to improve the quality of the creek environment, and provide habitat and biodiversity connections.





Health and Civic Precinct

This precinct runs along a major entrance road to Gisborne and provides a transition from the rural landscape setting to the town centre. Travelling down Bacchus Marsh Road, the first glimpses of built form and town are available at the bend in the road to the west of Dixon Field. The built form character along Robertson Street is currently low-scale, modest and recessive to the open space, escarpment views and street tree planting. Development along this road will influence the 'first impression' for travellers entering town.

The precinct is bound by Hamilton Street to the north, a secondary entrance road with views that extend above the existing, low-scale built form to the Jacksons Creek escarpment and Macedon Ranges beyond. These views provide a connection to the broader landscape setting of the township and reinforce the setting of Gisborne as a 'village in a valley.'

The Gisborne Future Structure Plan contains the objective to:

Ensure new development does not detract from the highly-valued character of Gisborne's landscape setting.

This is to be achieved through the following strategies and actions:

- Minimise the visual intrusion and insensitive siting and design of built form and infrastructure on the entrances and gateways to Gisborne.
- Reinforce town centre entry points and key intersections with high quality built form and landscape treatment that provides a clear signal of entry.
- Avoid insensitive or branded built form, lighting, billboards and signage that could detract from the quality of township entrances.

Development Requirements

The general guidelines provided in the UDF apply. In addition to these, the following site-specific design requirements apply:

- Street wall heights of 8.5m apply to the Robertson, Neal and Hamilton Street interfaces.
- A two storey height limit is preferred across the Hamilton Street frontage to maintain views to the escarpment and Macedon Ranges beyond.
- Buildings are to be designed to respond to sloping conditions, and are to be stepped down sites to avoid large areas of sheer wall.
- A two storey maximum height limit is applied along the Robertson Street frontage to maintain the low-scale character of the town at the transition from rural landscapes to township built form.
- A landscaped street setback of 5-10m is required to all road interfaces to enhance the quality of wide, tree-lined road reserves and ensure adequate space is provided for landscaping around built form.
- Heights of 3-4 storeys may be achievable within internal areas in the precinct. Higher built form will be required to meet town centre design objectives, and demonstrate that opportunities for outward views are maintained through breaks in upper levels of built form, as viewed from Hamilton Street.
- Any upper levels are to be setback 5m behind the street wall to ensure built form is not visually intrusive.
- Car parking and vehicle access is to be provided from consolidated access points. Crossovers to individual sites or buildings are discouraged.
- Development is to address street frontages through windows, openings and pedestrian access.

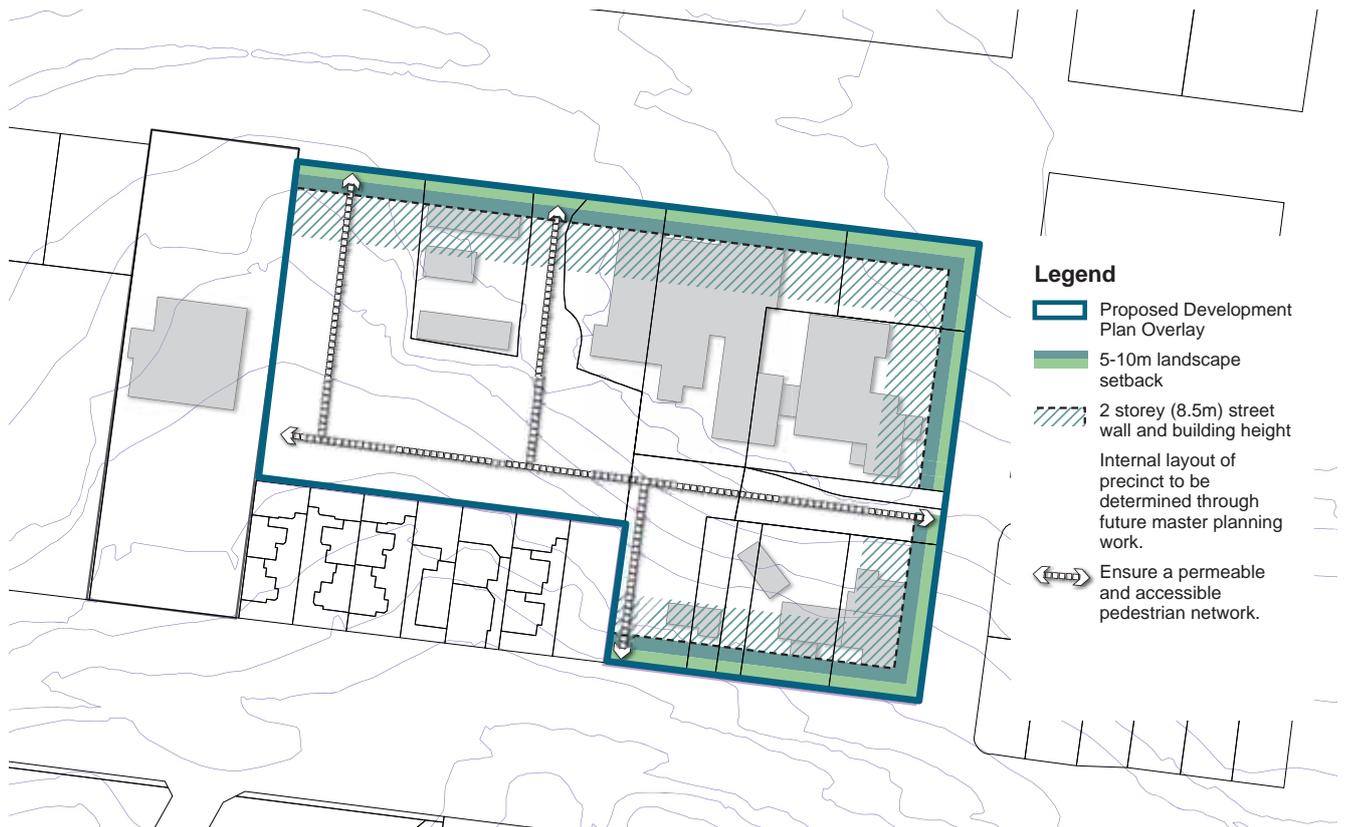
There are large areas of vacant land within the precinct beyond the street frontage that have capacity for a greater level of development.

Master planning work is required to determine site access, open space and built form arrangements and appropriate zoning.

It is proposed that a Development Plan Overlay be applied to this precinct to ensure that the future development of this land occurs in a coordinated manner, and that a 'whole of site' approach is prepared in consultation with all affected parties.



Health Precinct



7. Landscape & Public Realm



Create attractive civic spaces with landscaping and amenities that exhibit high quality design and provide opportunity for outdoor interaction and passive use of the street.

7.1. Trees & Landscape Setting

Gisborne’s street trees and open spaces have local heritage values, with some trees up to 130 years old. The trees along with Gisborne’s heritage buildings, give the town it’s distinct character. It is important that trees are maintained and valued for their contribution to the spacious, semi-rural character of town.

With rising temperatures due to climate change, a mature canopy cover has a cooling effect on streets and a positive impact on physical and visual amenity. Studies have shown that shoppers will stay in areas longer with a mature tree canopy than one without. Trees improve public health by cleaning the air; alleviating stress; they promote outdoor walking; and provide buffers for noise and pollution.

- Any development needs to consider key aspects of township character and historical landscape particularly significant trees with high retention value within roadsides, streetscapes and open space reserves.
- Protect and retain the significant trees that line streets within the Gisborne town centre and Station Road, New Gisborne.
- Ensure works within streetscapes and roads are planned to increase canopy cover within the town centre.





- Open space & reserves
- Sports Fields
- Existing trees
- Waterway

Figure 12. Landscape Setting



- Protect, retain and enhance Gisborne's significant trees, avenues and boulevards
- Opportunities to improve tree planting within streetscapes.

Figure 13. Opportunities for Street Trees

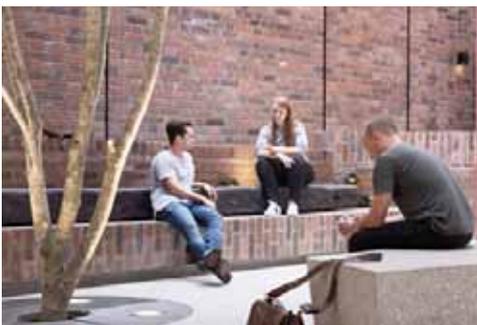
7.2. Public Places

There is opportunity to increase the amount of landscaping and planting within Gisborne's streetscapes. The inclusion of Water Sensitive Urban Design (WSUD) where appropriate will improve the quality of stormwater runoff and passively irrigate garden beds and trees.

The future development of Gisborne's streets should occur in a coordinated way that ensures that developments deliver improvements to their adjacent public realm.

Streetscape Design Guidelines

- 1 Provide raised pedestrian thresholds at intersections to improve pedestrian legibility and create a visual cue for cars to slow down.
- 2 Allow for generous kerb outstands and civic spaces at intersections. These should include 'pause points' with seating and pavement materials and public art that gives the space a sense of identity.
- 3 Allow for the retention of existing trees and opportunities for new tree planting
- 4 Create landscape areas that define and enhance the street, incorporate WSUD where possible.
- 5 Provide wide pavements (minimum 5m, with 2m 'pedestrian zone') in front of any new commercial or retail development to cater for high pedestrian flows, outdoor dining and other street activity.
- 6 Locate street furniture and pause points in areas that are visible, close to pedestrian desire lines and gathering spaces.



Closure of slip lanes could allow for the creation of civic spaces in streetscapes (Project example: Keilor Village).



Water Sensitive Urban Design



Tree pits and raingardens within the streetscape passively irrigate trees and plants, treat stormwater and add visual interest and greenery to streetscapes. Images courtesy of Melbourne City Council.



Dawson Street raingarden, Sunshine treats run off from nearby car park and creates a landscape feature along a pedestrian link that interfaces a future development site. Image courtesy of Brimbank City Council.



The Gisborne WSUD Strategy (2015) identifies possible locations for the harvesting and treatment of stormwater within the Jacksons Creek open space corridor. The raingarden in the Edinburgh Gardens, North Fitzroy, provides an example of how this type of infrastructure can be both functional and aesthetically pleasing.



The raingarden treats stormwater which is then used to water trees and sporting fields within the Edinburgh Gardens, and creates an interesting and functional landscape feature as a point of interest within the park. Images courtesy of GHD / Landezine.

8. Movement and Access



Provide streetscapes that are safe, interesting, connected and comfortable for pedestrians to experience and encourage activity, economic prosperity and social interaction within the town centre.

The streets and public spaces in the Gisborne town centre should provide enjoyable, comfortable and engaging experiences for people.

Urban centres designed at a 'pedestrian scale' prioritise the movement, safety and comfort of pedestrians over vehicle movements. They include built form that can be easily 'read' as a pedestrian through the location of street elements, awnings and openings that engage a pedestrian at the street level.

Good quality public open spaces and streetscapes provide opportunities for people to meet and gather and provides opportunities for incidental interaction, which strengthens community cohesiveness and sense of place.

The design of urban environments can help deliver better health outcomes for the community. Research has shown that residents living within convenient walking distance of a variety of local living destinations, such as supermarkets, shops, parks and public transport are more likely to choose walking as a transport option than driving.

Active streets, trees and high quality public spaces encourage business investment and innovation within urban spaces and centres.

8.1. Design for Pedestrian & Cyclist Priority

It is important that roads and footpaths within the town centre are designed to prioritise pedestrian and cyclist movements. Road and streetscape upgrade projects are to provide:

- Raised pedestrian thresholds at key intersections that provide visual cues for pedestrian priority through changes in material treatment, levels and elevated pavements that improve access for those with limited mobility, parents with prams or people with shopping carts.
- Formalised crossings that give pedestrians right of way at roundabouts;
- Kerb out-stands that bring pedestrians out from behind parked cars and reduce the distance pedestrians need to travel to cross a road;
- Pedestrian refuges in central medians of main roads;
- Pedestrian amenities including 'pause-points' with seating, landscaping and shade along key routes.
- A nominal amount of on-street parking to enhance surveillance and encourage activity on the street, ensuring that it does not compromise pedestrian amenity.
- Closure of slip lanes and creation of civic spaces.



Raised pedestrian threshold example, image courtesy of Hobart City Council



Pedestrian crossings at roundabouts, image courtesy of City of Greater Bendigo.

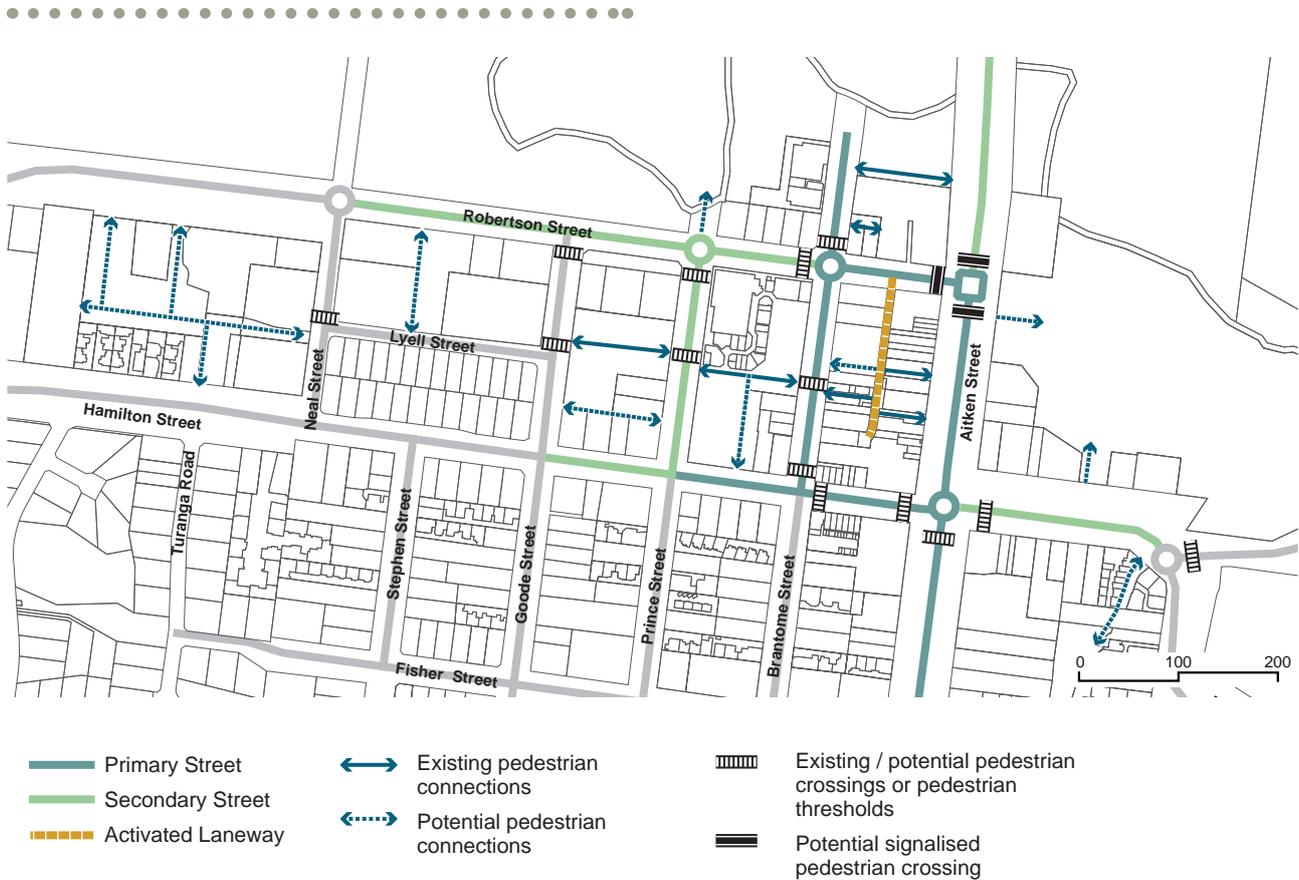
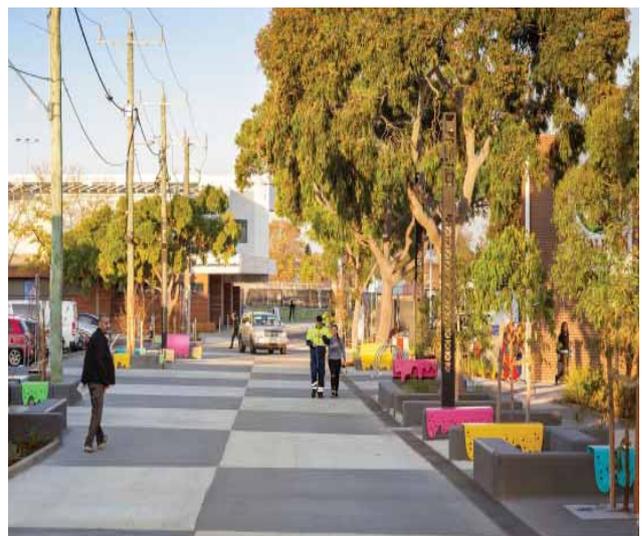


Figure 15. Pedestrian Connections

Key pedestrian links that will be important for people moving throughout the town centre are illustrated in Figure 10.

Delivery of these should be provided in line with new development and through streetscape upgrades.

- New development is to maintain or provide active laneways, connections and arcades to reinforce a permeable and connected pedestrian network throughout the town centre.
- These are to be safe, attractive, open to daylight and publicly accessible with active frontages, good lighting and a clear line of sight from one end to the other.
- Pedestrian connections are to align with existing connections and road alignments to create a town centre that is legible and easy to walk around.
- Spaces for bins and other storage are to be appropriately screened to ensure a tidy presentation onto laneways and parking areas.



Princess Street, St Albans. Shared user laneway environment. Image courtesy of SVC.

8.2. Streetscape Concept Plans

The following streetscape concept plans illustrate how recommendations to improve the function, safety and amenity of roads and streetscapes in the Gisborne town centre may be achieved to inform future planning work, and to guide future design in these areas. This is to be read in conjunction with the “Movement and Transport” Section of the Gisborne Futures Structure Plan.

Aitken Street

Currently a significant number of service access points off Aitken Street impact pedestrian amenity and create vehicle conflict points. There is also very poor pedestrian connectivity across both the service roads and the main carriageway. Concept plans provide a number of suggestions to improve conditions for both vehicles and pedestrians, while generally maintaining current levels of car parking. Improvements on Aitken Street will also enhance access to the community and sports precinct.

Brantome Street

Brantome Street will maintain its emphasis on active retail frontage and local street feel.

Robertson Street

A range of potential improvements have been identified along Robertson Street that consider both the strategic importance of the road and urban realm that will encourage pedestrian activity to the active frontages proposed for the street.

Hamilton Street

Hamilton Street provides a secondary east-west link between Bacchus Marsh Road and Aitken Street, enabling access to the health, retail and commercial precincts. Improvements to streamline service road access; pedestrian; and cyclist access should be considered to reflect the importance of this link in the town centre street network.

Heritage Way

It is intended to improve Heritage Way from its current use as a service laneway to a more active, pedestrian-focused environment. While service vehicle access will still be required, a range of improvements will encourage pedestrian activity and safety.

Primary Cycling Routes

On-road bicycle lanes are to be provided on all roads identified as a Primary Cycling Routes in Figure 15.

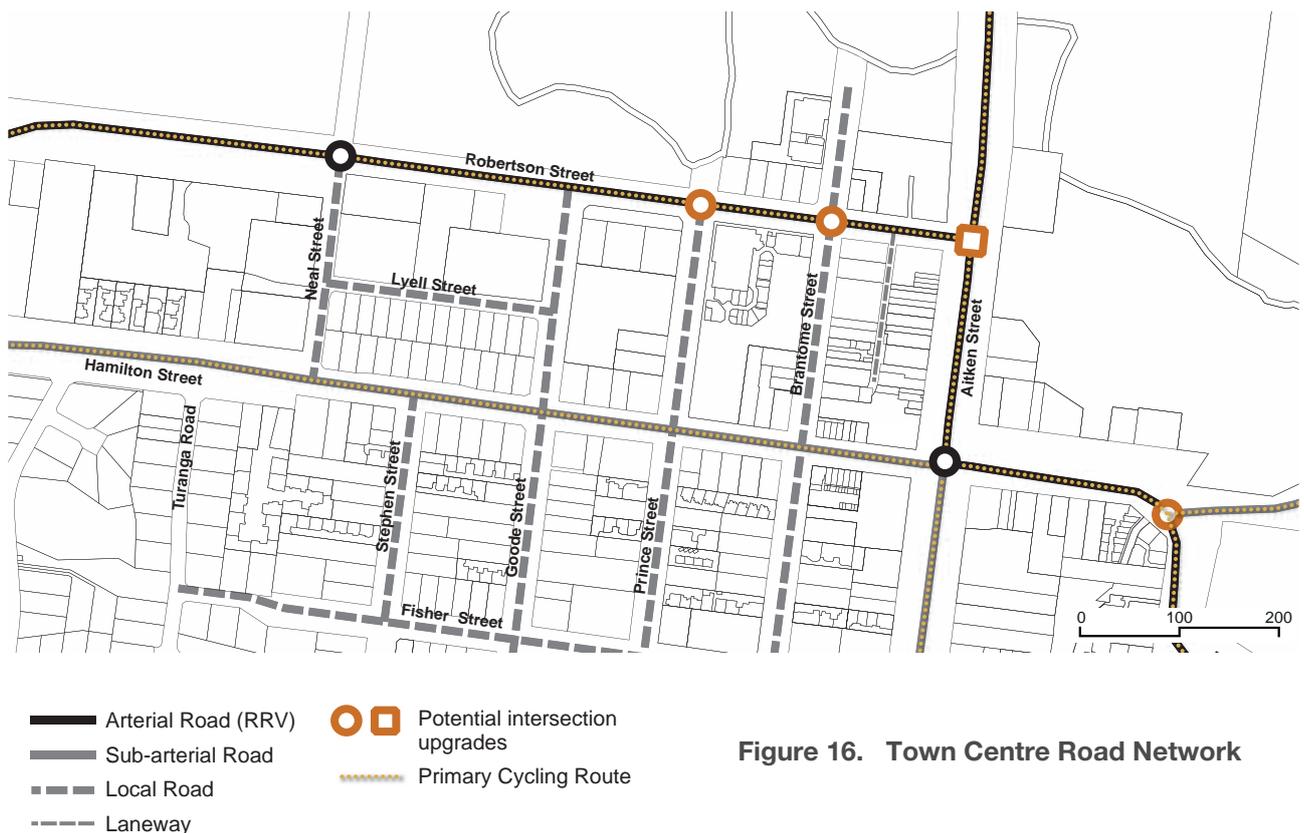


Figure 16. Town Centre Road Network

- 1 Close slip lane access points and create civic spaces with opportunity for outdoor dining, seating and public realm improvements.
- 2 Signalise intersection to improve traffic flow and provide pedestrian access.
- 3 Close slip lane access to improve pedestrian amenity.
- 4 Provide on-road cycle lanes on Aitken Street.
- 5 Introduce planting to median islands to soften the road environment.
- 6 Close vehicle access to service road and improve pedestrian connection and landscaping to Gardiner Reserve.
- 7 Relocate service road access and allow two-way access to Gardiner Reserve.
- 8 Increase street tree planting within Aitken Street service road.
- 9 Consider closure of mid-block service lane access to minimise vehicle conflicts and create additional car parking.
- 10 Any future road works in Aitken Street must retain significant trees, wide landscaped road reserves and prioritise pedestrian access.
- 11 Formalise car parking in Gardiner Reserve.
- 12 High priority footpath upgrade.



Figure 17. Aitken Street Concept Plan



- 1** Close slip lane access points and create civic spaces with opportunity for outdoor dining, seating and public realm improvements.
- 2** Relocate access to Aitken Street service lane and create additional parking.
- 3** Provide raised pedestrian thresholds with zebra crossings at arms of Hamilton Street roundabout. Ensure thresholds are bus capable.
- 4** Provide on-road cycle lanes on Hamilton & Aitken Streets.
- 5** Introduce shared-user zones within service road to enhance the pedestrian environment and provide temporal event spaces.
- 6** Increase tree planting within streetscapes along Hamilton Street and Aitken Street.



Figure 18. Hamilton Street Roundabout Concept Plan



Closure of slip lanes creates opportunities for pedestrian pause points and small civic spaces.



WSUD tree pits may be used within existing footpaths to increase opportunities for tree planting, particularly along the west side of Aitken Street and south side of Hamilton Street.



- 1 Provide right-hand turn lanes along Robertson Street.
- 2 Potential future intersection upgrade
- 3 Provide on-road cycle lanes on Robertson Street.
- 4 Future development to provide pedestrian streetscape with built form to front boundary, street tree planting and on-street parking.
- 5 Consolidate car parking and service access for future development to a single crossover on Brantome Street.
- 6 Potential for taller planting within existing landscape setback to soften blank wall and frame mural on side walls of Gisborne Village Shopping Centre.
- 7 Provide pedestrian refuge (short term).
- 8 Potential relocation of pedestrian crossing (long term).
- 9 Introduce median islands with planting to soften the road environment.
- 10 Consider landscaping and access improvements as part of masterplan for Jacksons Creek corridor.
- 11 Future development to provide an attractive, active interface to parkland.
- 12 Provide raised pedestrian thresholds at intersections.
- 13 Underground powerlines as part of new development.

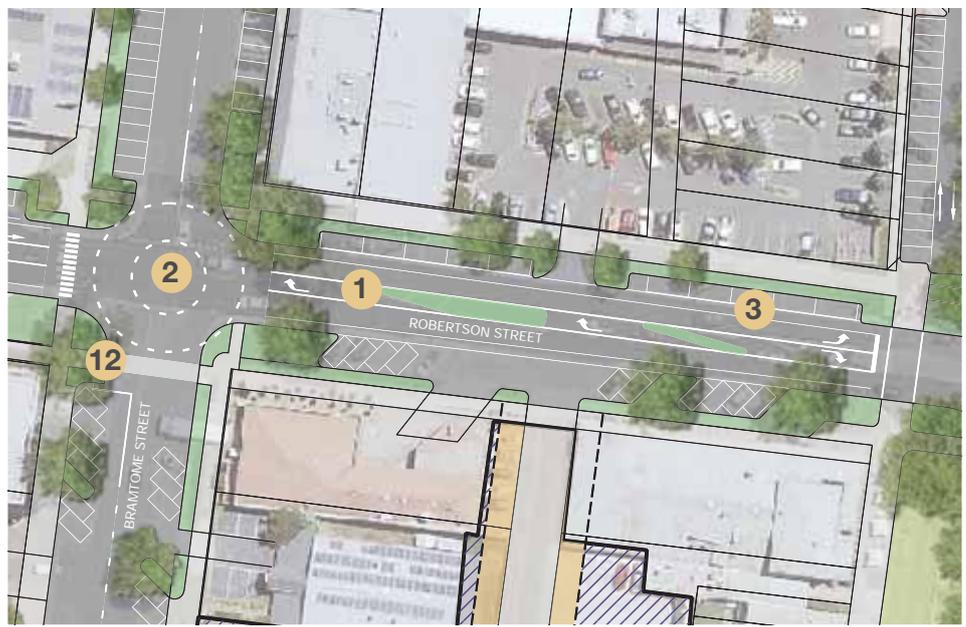


Figure 19. Robertson Street Concept Plan (between Brantome & Aitken Streets)

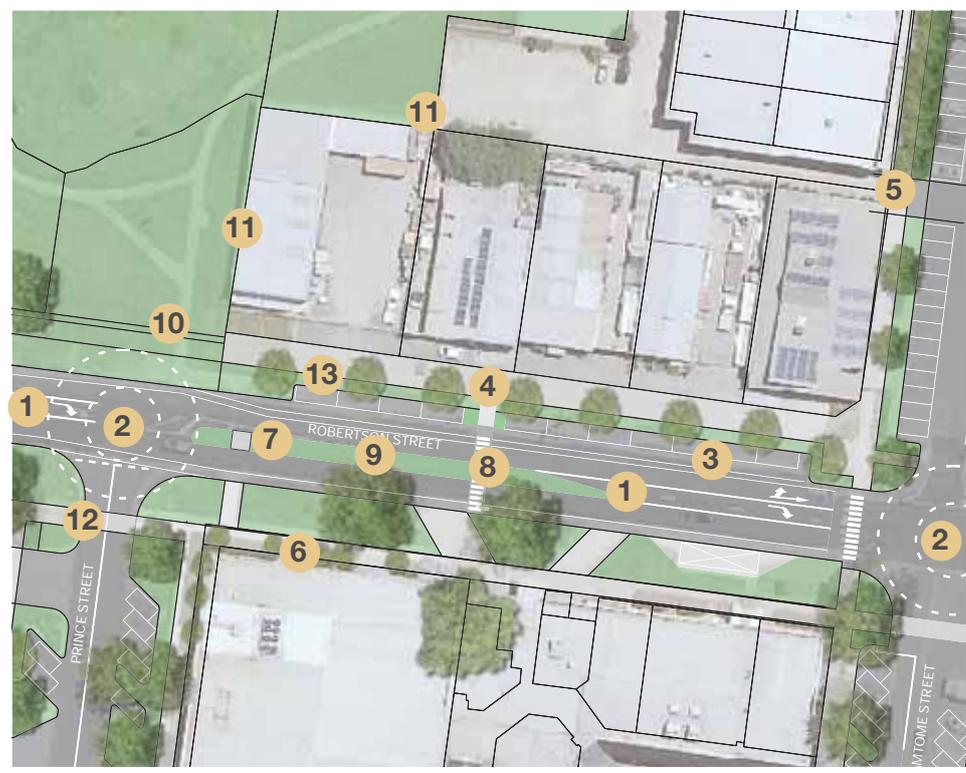


Figure 21. Robertson Street Concept Plan (between Prince & Brantome Streets)



- 1 Provide right-hand turn lanes along Robertson Street, ensure access for emergency services is maintained.
- 2 Provide on-road cycle lanes on Robertson Street.
- 3 Formalise long vehicle and truck parking along Robertson Street.
- 4 Provide pedestrian refuge.
- 5 Provide raised pedestrian thresholds at intersections.
- 6 Consider rest stop facilities, landscaping and access improvements as part of masterplan for Jacksons Creek corridor.
- 7 Introduce median islands with planting to soften the road environment.
- 8 Formalise edges and provide car parking on Robertson Street between Prince and Goode Streets.
- 9 Increase tree planting and strengthen Robertson Street boulevard.



Figure 23. Robertson Street Concept Plan (between Goode & Prince Streets)



Figure 25. Robertson Street Concept Plan (west of Goode Street)

8.3. Car Parking

Car Parking Supply and Demand

Car parking surveys have been undertaken to determine existing parking restrictions and level of demand within Gisborne town centre.

Currently there are 1,436 existing car spaces within areas surveyed (refer Figure 10) for use by commercial employees, visitors and residents which includes 757 on-street and 679 off-street car spaces.

Existing parking supply within Gisborne town centre is mostly either unrestricted parking or two-hour parking. Unrestricted parking spaces are provided predominantly off-street, whilst two-hour parking spaces are located predominantly on-street.

Car parking occupancy surveys indicate that when car parking demand within the Gisborne town centre is at its highest, approximately 1 in 3 public parking spaces, in both on- and off-street locations, remain vacant.

Planning Scheme Considerations

- The planning scheme requires that car parking spaces are to be delivered on-site by the developer. Reductions in car parking may be negotiated between the developer and Council. Any request for a reduction must be accompanied by a Car Parking Demand Assessment Report.
- Consideration may be given to the hours of operation and use, and whether car parking spaces may be shared between uses.
- Car parking supply and demand will require ongoing monitoring to ensure supply remains adequate in the town centre.
- If it becomes evident that car parking demand is not met by supply then a detailed Car Parking Plan may be required, and it may be necessary to formalise changes to the car parking rates required by the planning scheme through a Car Parking Overlay.
- A Car Parking Overlay may also include provision for developers to contribute to public car parking infrastructure as an offset to on-site parking, particularly for constrained sites where meeting car parking requirements may be prohibitive to the future development of the site.



Figure 27. Car Park Survey



8.4. Car Parking Strategies & Actions

- Enhance pedestrian amenity, safety and connectivity within the centre to encourage people to walk to destinations.
- Ensure new car parking proposals do not compromise the quality of pedestrian, cycle and landscape infrastructure.
- Regularly monitor car parking supply and demand to ensure that supply remains sufficient.
- Provide sufficient disability parking and regularly monitor to ensure supply meets rising demand as the population ages – particularly in the health and civic precinct.
- Introduce additional time-limited parking in commercial and retail areas to encourage staff to park in unrestricted parking areas further away from areas where short-stay parking is in high demand.
- Provide additional or improved wayfinding signage to direct vehicles to underutilised car parking areas.
- Ensure there is no net-loss of car parking spaces as part of new development, particularly where built form is proposed to replace existing surface car parks.
- Multideck car parking structures above and/or below ground may be a long term solution to parking provision, however any multideck structure must be sleeved with retail, commercial or residential uses, provide active street frontages at the ground floor and allow for future adaptable re-use of the building.

8.5. Car Park Design Guidelines

- Surface car parking is not permitted in the front setback of new developments in the town centre.
- Car parking access to be consolidated to secondary streets and laneways where possible.
- Large areas of impervious surface in car parks contribute significantly to stormwater and pollutant loads. Raingardens or bio-filtration trenches are to be provided in surface car parks to filter stormwater, create landscape features that delineate boundaries between the car park and adjacent streets, and support tree planting for visual amenity and shade.
- Car parking areas are to provide clear and safe pathways for pedestrians, and be legible for both vehicle and pedestrian movements.
- Car parks are to be offset from boundaries and edges for a minimum of one metre to allow for landscaping, WSUD and screening planting.
- One canopy tree is to be provided per six car parking spaces in new car park areas.
- Use of recycled materials in asphalt mixes is encouraged.
- Where internal car parking does interface with the street, it is to be predominately screened from view through sleeved built form and generous landscaping.
- Restricting the location of vehicle access to car parks away from the public realm to reduce the chance of conflict between vehicles and pedestrians.
- A 600mm landscape edge is to be provided between any carparking adjacent to footpaths to account for bonnet overhang.

9. Implementation Summary

It is proposed that the Gisborne Futures Urban Design Framework will be implemented into the planning scheme through the introduction of a Design and Development Overlay to the Gisborne town centre, and the application of the Development Plan Overlay to key development sites.

It is proposed that a Local Policy for signage is prepared.

The UDF will also be used as a guide for future capital works, subject to funding availability and detailed design.

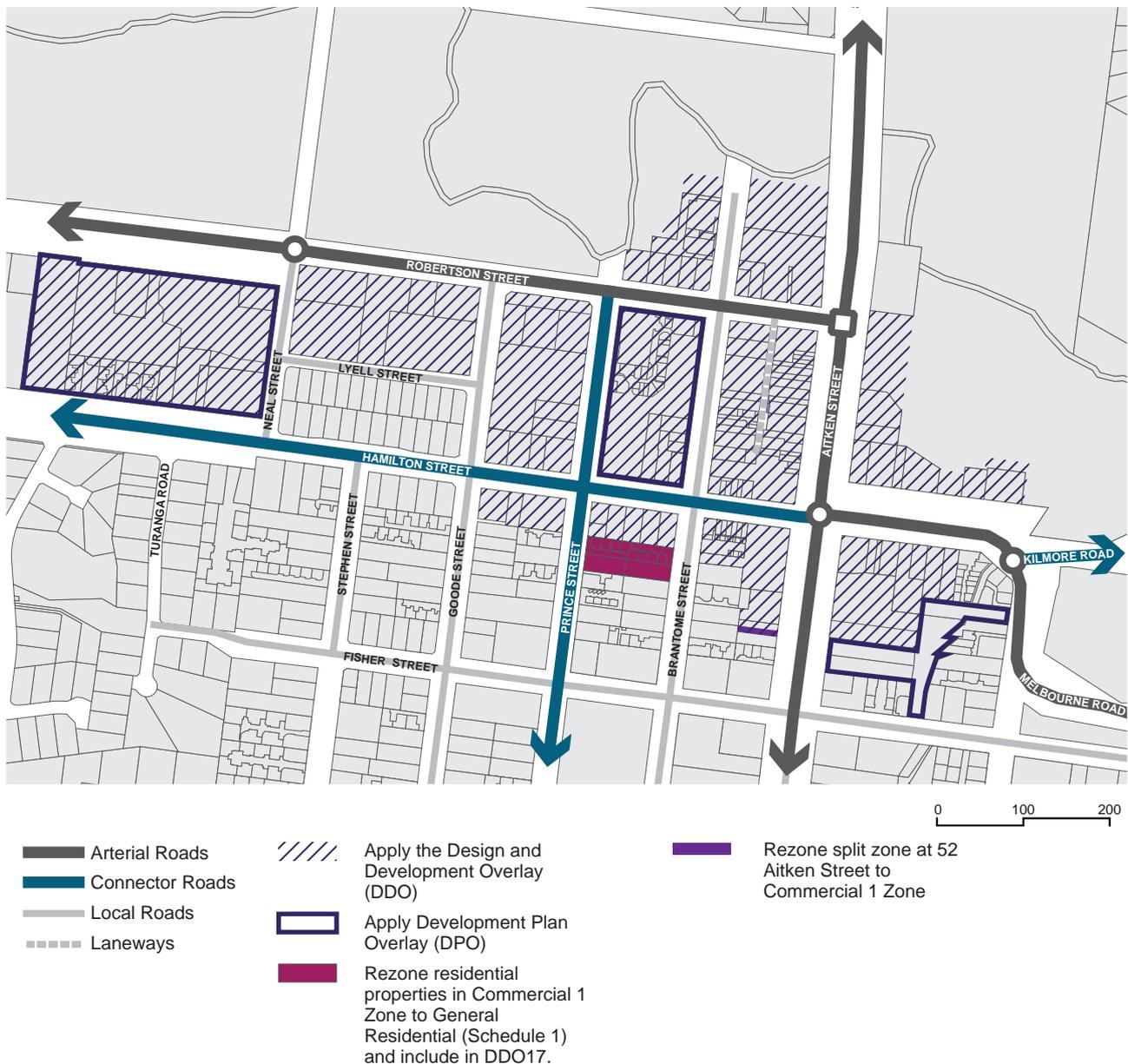


Figure 28. Implementation Summary

10. Next Steps

10.1. Community Consultation

The draft documents that comprise the Gisborne Futures project will be presented to the community and other stakeholders for comment and feedback for period of 7 weeks.

The findings of the consultation period will be carefully considered and will inform the final drafts of the plans and an implementation plan will be prepared.

A second round of community consultation on the draft plans will be undertaken before the plans are finalised.

10.2. Implementation

An Implementation Plan will be prepared that builds on the strategies and actions highlighted throughout the Gisborne Futures plans to provide a framework for delivering the vision for Gisborne.

The Implementation Plan will be used as a guide to identify Council's role, responsibilities and priority for each recommended action.

10.3. Final Draft Reports

The Gisborne Futures plans will be redrafted after considering comments and feedback from the community and stakeholders.

A report that considers submissions received/ issues raised and that recommends further amendments to the Gisborne Futures plans will be presented to the Council for consideration and endorsement.

Pending Council endorsement, necessary amendments to documentation (where required) will be carried out to facilitate the preparation of the Final Structure Plan, Urban Design Framework and Neighbourhood Character Study. These final reports will inform future decisions in relation to local planning policy and controls.

Council's Role

Macedon Ranges Shire Council will play different roles in the implementation of the Gisborne Futures project. These will vary between the roles of Planner, Provider, Advocate, Partner/ Facilitator, Educator and Regulator. A description of these various roles is provided below.

Planner

Develop detailed plans and drawings for construction, and in relation to its urban and social planning responsibilities

Advocate

Represent community needs and interests to Federal and State Governments and the private sector

Partner / Facilitator

Working closely with developers, landowners, residents and businesses

Educator

Provide information to businesses, residents and interest groups

Regulator

Ensure that built form, infrastructure and other elements of the environment meet town planning, building, transport and public health regulations and expectations.

10.4. Adoption & Planning Scheme Amendment

Implementation will involve seeking Council's formal adoption of the plan which will be followed by a planning scheme amendment process to implement key policy and direction into the Macedon Ranges Planning Scheme.





**Macedon
Ranges**
Shire Council